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SHEET NO.	REV.	DESCRIPTION
EC-STR-3B	04-01-08	SILT FENCE
EC-STR-3D	04-01-08	ENHANCED SILT FENCE
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
RD01-S-11	04-04-03	DESIGN AND CONSTRUCTION DETAILS FOR ROADSIDE SLOPE DEVELOPMENT
T-M-1	04-15-04	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	09-05-04	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-9	11-30-04	MARKING DETAILS FOR RAMP INTERSECTIONS
S-GR-11	11-26-07	W-BEAM & THRIE BEAM BARRIER RAIL AND RUB RAIL ALTERNATES
S-GR-12	05-27-03	W-BEAM BARRIER POST DETAILS AND SPECIFICATIONS
S-GR-13	05-27-03	BARRIER RAIL MOUNTING POST BLOCK-OUTS WITH VERTICAL ADJUSTMENT HOLES
S-GR-13A		BARRIER RAIL MOUNTING POST FOR PLACTIC BLOCK-OUTS WITH HORIZONTAL ADJUSTMENT HOLES
S-GR-14	09-05-98	W-BEAM BARRIER FASTENING HARDWARE AND BRIDGE APPROACH DELINEATORS
S-GR-15	06-30-05	W-BEAM BARRIER TERMINAL ELEMENT DETAILS
S-GR-38	05-15-08	DETAILS FOR CONSTRUCTION OF EARTH PAD FOR TYPE 38 GUARDRAIL END TERMINALS
S-GR-38A	06-30-05	DETAILS FOR CONSTRUCTION OF ALTERNATE EARTH PAD FOR TYPE 38 GUARDRAIL END TERMINALS
S-GR-43		TANGENTIAL GUARDRAIL TERMINAL ANCHOR (TYPE 38) POST LAYOUT AND ERECTON DETAILS
S-GR-44		TANGENTIAL GUARDRAIL TERMINAL ANCHOR (TYPE 38) (2 TUBE) GUARDRAIL ELEMENT POST AND ASSEMBLY DETAILS

**BEGIN PROJECT L.M. 7.19 (MM 74.24)
(WEST OF EXIT 74)**

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 2006 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

TDOT ROAD SP. SV. 2 MICHAEL I. RUSSELL, REG. 4
DESIGNER ROBERT JOWERS, REG. 4 CHECKED BY SCOTT PATE, REG. 4

P.E. NO. 98043-4283-04
PIN: 040942.01

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

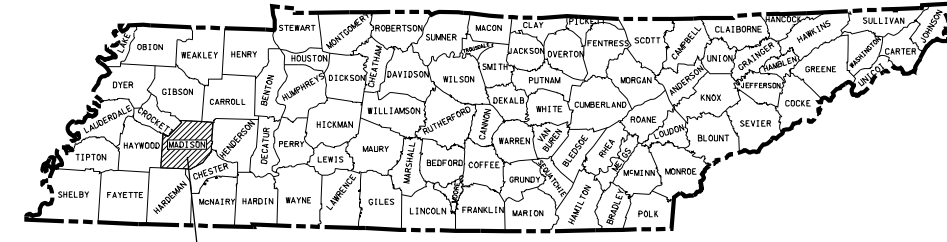
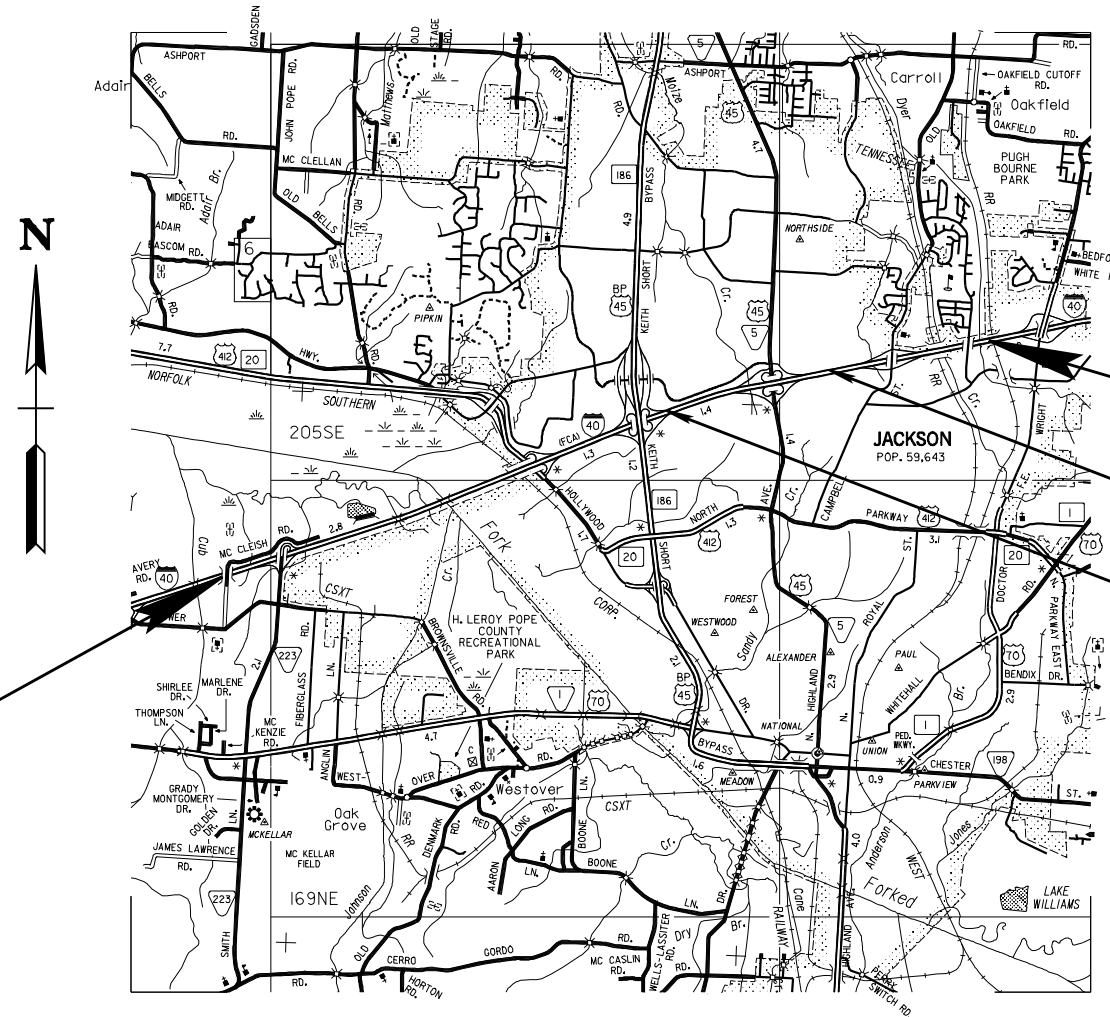
MADISON COUNTY

I-40

FROM: L.M. 7.19 (MM 74.24)(WEST OF EXIT 74)
TO: L.M. 16.64 (MM 83.69)(EAST OF EXIT 83)

RESURFACING

STATE HIGHWAY NO. F.A.H.S. NO. N.A



PROJECT SITE

EXCLUSION	
LOG MILE TO LOG MILE	LENGTH (MI.)
LM 13.59 (MM 80.64) TO LM 15.57 (MM 82.62)	= 1.98

**END PROJECT L.M.16.64 (MM 83.69)
(EAST OF LM 13.59 (EAST OF EXIT 83))**

**EXCLUSION FROM LM 13.59 (MM 80.64)
TO LM 15.57 (MM 82.62)**

APPROVED: Paul D. Degees
CHIEF ENGINEER

DATE: _____

APPROVED: Scott F. Nief
COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

TRAFFIC DATA	
ADT (2009)	38,559

SCALE: 0 1 2 3 MILES

PROJECT LENGTH 7.47 MILES

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
① 202-03.02	REMOVAL OF RIGID PAVEMENT	C.Y.	88
② 203-03	BORROW EXCAVATION (UNCLASSIFIED)	C.Y.	96
203-06	WATER	M.G.	21
② 209-08.03	TEMPORARY SILT FENCE (WITHOUT BACKING)	L.F.	1,200
③ 303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING "C" OR "D"	TON	2,708
④ 307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	242
307-03.08	ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING B-M2	TON	1,808
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	26
⑤ 403-05.01	BITUMINOUS MATERIAL (FOG SEAL) SHOULDER	TON	80
407-10.03	FIBER GLASS MAT (PAVEMENT REINFORCING)	S.Y.	16,000
⑥⑦⑧ 411-03.10	ACS MIX(PG76-22) GRADING D	TON	19,822
⑨ 415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	2,864
⑩ 415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	275,811
⑪ 705-04.07	TAN ENERGY ABSORBING TERM (NCHRP 350, TL3)	EACH	8
712-01	TRAFFIC CONTROL	LS	1
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	525
⑫ 712-06	SIGNS (CONSTRUCTION)	S.F.	3,578
712-08.03	ARROW BOARD (TYPE C)	EACH	4
713-16.03	CHANGEABLE MESSAGE SIGN	EACH	2
716-01.10	SNOWPLOWABLE REFLECTIVE MARKER	EACH	1,362
716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	1,362
716-02.02	PLASTIC PAVEMENT MARKING (8" BARRIER LINE)	L.F.	7,600
716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	325
⑬ 716-02.10	PLASTIC PAVEMENT MARKING (6" LINE)	L.M.	40.8
716-04.03	PLASTIC PAVEMENT MARKING (4" DOTTED LINE)	L.F.	3,750
⑭ 716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	40.8
717-01	MOBILIZATION	LS	1
② 801-03	WATER (SEEDING & SODDING)	M.G.	2.5
② 803-01	SODDING (NEW SOD)	S.Y.	1800

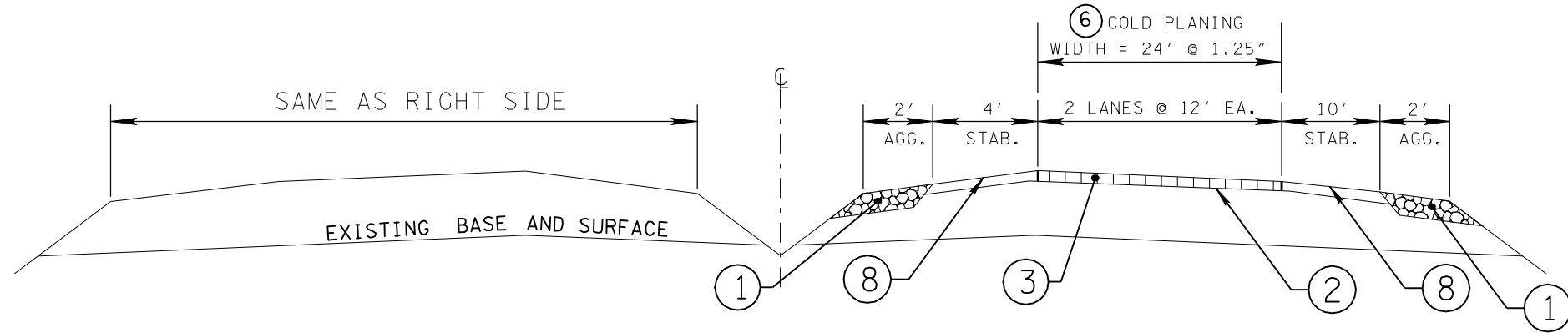
FOOTNOTES

- ① EXISTING ROADWAY PROFILE CONSISTS OF 9" P.C. CONCRETE.
- ② FOR GUARDRAIL TERMINAL EARTH BERM CONSTRUCTION.
- ③ FOR BREAKOUT AND LOW EXISTING SHOULDERS ONLY.
- ④ FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.02.
- ⑤ MATERIAL USED FOR THE FOG SEAL SHALL BE GSB88, RECLAMITE, TQF-1 OR OTHER DEPARTMENTAL APPROVED EQUIVALENT. THE MATERIAL SHALL COMPLY WITH MANUFACTURER REQUIREMENTS. THE MATERIAL AT TIME OF PLACEMENT SHALL BE IN ACCORDANCE WITH MANUFACTURER LITERATURE RECOMMENDATIONS.
- ⑥ INCLUDES 5736 TONS FOR RAMPS AND 150 TONS FOR SPOT LEVELING.
- ⑦ SEE SPECIAL PROVISION NO. 407G FOR MATERIAL TRANSFER DEVICE.
- ⑧ SEE SPECIAL PROVISION NO. 411B FOR RIDEABILITY SPECIFICATIONS.
- ⑨ COLD PLANE 300' OUT FROM EACH OF THE FIVE BRIDGES AT A DEPTH OF 3.25". SEE BRIDGE NOTES ON TYPICAL SECTION.
- ⑩ GENERAL COLD PLANE DEPTH FOR ROADWAY: 1/4". INCLUDES 86,576 S.Y. FOR RAMPS.
- ⑪ FOR UPGRADE OF EXISTING ANCHORS. THE COST OF REMOVAL OF EXISTING GUARDRAIL AND ANCHORS TO BE INCLUDED IN THIS ITEM.
- ⑫ QUANTITY INCLUDES
- | | |
|---|---|
| BETWEEN L.M. 7.19 TO L.M. 13.59

4 ROAD WORK NEXT 6.4 MI. (G20-1)
4 END ROAD WORK (G20-2A)
4 MERGE LEFT (W4-2)
4 MERGE RIGHT (W4-2)
68 SHOULDER DROP-OFF (W8-9a)
68 UNEVEN LANES (W8-11)
4 ROAD WORK 1 MILE (W20-1)
8 ROAD WORK AHEAD (W20-1)
4 ROAD WORK 1500 FT. (W20-1)
4 ROAD WORK 1000 FT. (W20-1)
4 ROAD WORK 500 FT. (W20-1)
4 RIGHT LANE CLOSED 1,500 FT. (W20-5)
4 RIGHT LANE CLOSED 1,000 FT. (W20-5)
4 RIGHT LANE CLOSED 500 FT. (W20-5)
4 LEFT LANE CLOSED 1,500 FT. (W20-5)
4 LEFT LANE CLOSED 1,000 FT. (W20-5)
4 LEFT LANE CLOSED 500 FT. (W20-5)
4 FLAGGER AHEAD (W20-7A)
4 FRESH OIL (W21-2)
28 SHOULDER WORK (W21-5) | BETWEEN L.M. 15.57 TO L.M. 16.64

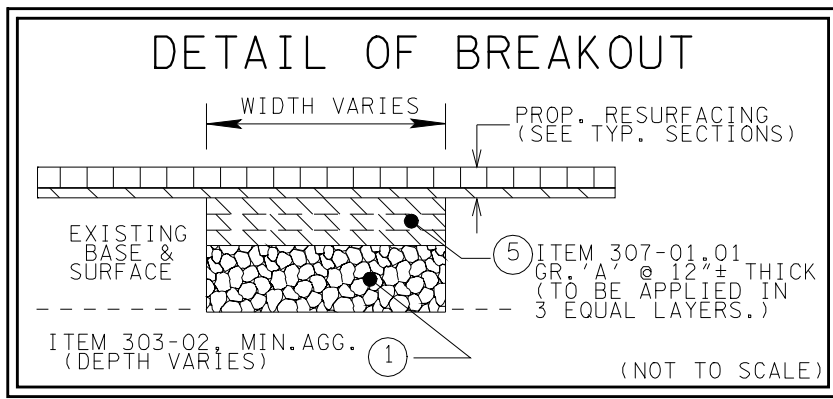
4 ROAD WORK NEXT 1.0 MI. (G20-1)
4 END ROAD WORK (G20-2A)
4 MERGE LEFT (W4-2)
4 MERGE RIGHT (W4-2)
12 SHOULDER DROP-OFF (W8-9a)
12 UNEVEN LANES (W8-11)
4 ROAD WORK 1 MILE (W20-1)
4 ROAD WORK AHEAD (W20-1)
4 ROAD WORK 1500 FT. (W20-1)
4 ROAD WORK 1000 FT. (W20-1)
4 ROAD WORK 500 FT. (W20-1)
4 RIGHT LANE CLOSED 1,500 FT. (W20-5)
4 RIGHT LANE CLOSED 1,000 FT. (W20-5)
4 RIGHT LANE CLOSED 500 FT. (W20-5)
4 LEFT LANE CLOSED 1,500 FT. (W20-5)
4 LEFT LANE CLOSED 1,000 FT. (W20-5)
4 LEFT LANE CLOSED 500 FT. (W20-5)
4 FLAGGER AHEAD (W20-7A)
4 FRESH OIL (W21-2)
4 SHOULDER WORK (W21-5) |
|---|---|
- THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.
- ⑬ FOR FINAL PAVEMENT MARKING ONLY. INCLUDES 7.2 MILES FOR RAMPS.
- ⑭ FOR TEMPORARY MARKING IF FINE TOOTH MILING IS PERFORMED. INCLUDES 7.2 MILES FOR RAMPS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	57201-8134-44	3



TYPICAL SECTION

FROM: L.M. 7.19 (M.M. 74.24)(WEST OF EXIT 74) TO: L.M. 13.59 (M.M. 80.64)(EAST OF EXIT 80B)
FROM: L.M. 15.57 (M.M. 82.62)(WEST OF EXIT 83) TO: L.M. 16.64 (M.M. 83.69)(EAST OF EXIT 83)



BRIDGE NOTE

THE CONTRACTOR SHALL:

COLD PLANE BRIDGE DECK 1.25 INCHES OF THE EXISTING ASPHALT (ITEM 415-01.02) AND REPLACE WITH 1.25 INCHES OF 411-D SURFACE MIX (3) FOR EB AND WB STRUCTURES AT L.M. 11.90 AND L.M. 8.35.

COLD PLANE ROADWAY APPROACHES FROM THE ENDS OF EACH STRUCTURE OUT 300 FT. (ITEM 415-01.01) TO A TOTAL DEPTH OF 3.25 INCHES (L.M. 8.35, L.M. 10.48, L.M. 11.08, L.M. 11.90, & L.M. 12.00). REPLACE WITH 2" LIFT OF BM-2 BINDER (4) AND 1.25" 411-D SURFACE (3).

APPLY FIBERGLASS / FIBEROUS MAT REINFORCEMENT BETWEEN BM-2 BINDER AND 411-D SURFACE LAYERS. ACCEPTABLE REINFORCEMENT PRODUCTS SHALL CONSIST OF GEOGRID 8501 OR 8511, PETROMAT, HATELIT ASPHALT REINFORCEMENT, OR OTHER APPROVED EQUALS AS DETERMINED BY THE DEPARTMENT. THE UNIT COST FOR FIBERGLASS / FIBEROUS MAT REINFORCEMENT (ITEM NO. 407-10.03 FIBER GLASS MAT (PAVEMENT REINFORCING, PER SQUARE YARD) SHALL INCLUDE THE COST FOR ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS, INCLUDING BUT NOT LIMITED TO ALL ASPHALTIC TACK COATING REQUIRED TO PROPERLY INSTALL THE PRODUCT IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDED INSTALLATION PRACTICES.

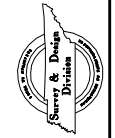
PROPOSED PAVEMENT SCHEDULE

<p>① MINERAL AGGREGATE BASE @ 2"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D" (THIS ITEM IS TO BE USED FOR BREAKOUT AND LOW SHOULDERS ONLY)</p>	<p>⑤ BITUMINOUS BASE COURSE (BLACK BASE) @ 12"± THICK (APPROX. 1,380.0 LBS./S.Y.) ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "A" (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)</p>
<p>② TACK COAT (TC) ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (AT 0.02 GAL./S.Y.)</p>	<p>⑥ COLD PLANING @ 1.25"± THICK (APPROX. 131.25 LBS./S.Y.) ITEM 415-01.02 COLD PLANING BITUMINOUS PAVEMENT</p>
<p>③ ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25" THICK (APPROX. 132.50 LBS./S.Y.) ITEM 411-03.10 ASPHALT CEMENT (76-22)(ACS) GRADING "D"</p>	<p>⑦ COLD PLANING @ 3.25"± THICK (APPROX. 341.25 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT (THIS ITEM IS TO BE USED FOR BRIDGE APPROACHES ONLY)</p>
<p>④ BITUMINOUS COURSE (BINDER) @ 2"± THICK (APPROX. 226.0 LBS./S.Y.) ITEM 307-03.08 ASPHALT CONCRETE MIX (PG76-22)(BPMB-HM) GRADING "B-M2" (THIS ITEM IS TO BE USED FOR BRIDGE APPROACHES ONLY)</p>	<p>⑧ BITUMINOUS MATERIAL (FOG SEAL) SHOULDER ITEM 403-05.01 BITUMINOUS MATERIAL FOR SHOULDER FOG SEALING (AT 0.10 - 0.15 GAL./S.Y. AS SPECIFIED BY ENGINEER)</p>

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
(NOT TO SCALE)

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GENERAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	57201-8134-44	4

TENNESSEE D. O. T.
DESIGN DIVISION
FILE NO.

GRADING

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE, OR LOCAL AGENCY.

GUARDRAIL

THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPE UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE A LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL.

UTILITIES

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

MISCELLANEOUS

NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS BEFORE DARK HOURS. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01 PAINTED PAVEMENT MARKINGS (4" LINE), LIN. MI.

IF THERMOPLASTIC IS USED ON THE FINAL SURFACE, THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKING AFTER THE PAVING OPERATION IS COMPLETED. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-02.10 PLASTIC PAVEMENT MARKING (6" LINE), LIN. MI.

PAVEMENT - RESURFACING

THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL NOTES

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS THIRTY (30) FEET SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY. WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC. UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS THIRTY (30) FEET SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL

DIFFERENCE IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC. CAUSED BY BASE, PAVING OR RESURFACING:

DIFFERENCE IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.

- A. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- B. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- C. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- D. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN 7 WORKDAYS AFTER THE CONDITION IS CREATED.

EROSION CONTROL

ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE.

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFF-SITE MIGRATION OR DEPOSIT OF SEDIMENT ON ROADWAYS USED BY THE GENERAL PUBLIC.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

SPECIAL NOTES

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.

ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. COST TO BE INCLUDED IN ITEM NO. 712-06.

IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION. THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.01.

ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

THE CONTRACTOR WILL BE REQUIRED TO CLEAN AND BLADE GRASS FROM SHOULDERS BEFORE PLACING ITEM 303-02. MATERIAL BLADED FROM SHOULDERS TO BE PICKED UP AND DISPOSED OF AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN ITEM 303-02.

THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES OPEN TO TRAFFIC DURING NON-WORK HOURS / OR NON-WORK DAYS.

ONLY 4 ARROW BOARDS WILL BE PAID FOR DURING THE LIFE OF THIS PROJECT. IF THE CONTRACTOR'S WORK PHASES REQUIRED ADDITIONAL ARROW BOARDS, THEIR COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.

THE PROJECT WILL BE CONSTRUCTED USING TWO WORK ZONES
 FROM: L.M. 7.19 TO L.M. 13.59
 FROM: L.M. 15.57 TO L.M. 16.64

THE CONTRACTOR MAY ONLY WORK IN ONE WORK ZONE EASTBOUND AND ONE WORK ZONE WESTBOUND AT ANY TIME.

THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS TO ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 48 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED. EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.

THE RAISED REFLECTOR SHALL BE REMOVED BEFORE COLD PLANING.

WITH REGARD TO SEEDING OF THE SIDE SLOPES, THE CONTRACTOR SHALL INSURE THAT THE TOP THREE INCHES OF SOIL ON SLOPES IS SUITABLE FOR PROMOTING AND SUSTAINING GROWTH AND VEGETATION. THE COST SHALL BE INCLUDED IN THE COST FOR OTHER ITEMS.

THE DEPARTMENT SHALL RESERVE THE RIGHT TO REOPEN LANES AS NECESSARY WHEN TRAFFIC CONDITIONS ARE DEEMED UNACCEPTABLE (EXCESSIVE QUEUE LENGTHS AND DELAY TIMES). THE CONTRACTOR SHALL BE REQUIRED TO FULLY COOPERATE WITH THE PROJECT SUPERVISOR WHEN REQUESTED TO MAKE CHANGES TO THE TRAFFIC CONTROL.

MESSAGE BOARDS SHALL BE USED NEAR INTERCHANGES AND/OR OTHER DESIGNATED AREAS IN ADVANCE OF THE WORK ZONE TO ALERT MOTORISTS OF POSSIBLE DELAYS AND RECOMMEND THE USE OF ALTERNATE ROUTES. THE MESSAGES SHALL BE UPDATED AS OFTEN AS NECESSARY SO THAT THE MOTORISTS OBTAIN CURRENT TRAFFIC INFORMATION. MESSAGE BOARDS SHALL BE RELOCATED AS NECESSARY AS THE WORK PROGRESSES. THE CONTRACTOR SHALL BE REQUIRED TO IDENTIFY AN INDIVIDUAL WORKING ON THE PROJECT THAT WILL BE RESPONSIBLE FOR KEEPING THE MESSAGES CURRENT AND RELOCATING MESSAGE BOARDS AS REQUESTED BY TDOT. THE PROJECT SUPERVISOR SHALL HAVE THE AUTHORITY TO APPROVE ALL MESSAGES AND REQUIRED CHANGES AT ANY TIME DUE TO CHANGING TRAFFIC CONDITIONS.

TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITION ONLY:

- A: THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/4" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.
- B: THE SURFACE SHALL BE SWEEPED AND CLEANED ON ALL LOOSE MATERIALS.
- C: THE DIFFERENCE IN ELEVATION BETWEEN THE MILLED SURFACE AND THE ADJACENT LANE SHALL NOT EXCEED 1/2 INCHES.
- D: THE MILLED SURFACE SHALL BE PAVED WITHIN 48 HOURS.
- E: RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.
- F: ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGN (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS.
- G: IF RAVELING OR DETERIORATION OF THE MILLED SURFACE IS OCCURRING WHILE TRAFFIC IS DRIVING ON THE MILLED SURFACE, THEN THIS PRACTICE WILL NOT BE ALLOWED AND PAVING SHALL BE COMPLETED IMMEDIATELY AFTER MILLING.
- H: ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.

WORK SHALL BE PERFORMED BETWEEN THE HOURS OF 7:00 P.M. AND 5:00 A.M. DURING WEEKDAYS AND WEEKENDS. THE CONTRACTOR SHALL MAINTAIN ALL LANES OF TRAFFIC IN EACH DIRECTION BETWEEN 5:00 A.M. AND 7:00 P.M. SUNDAY THROUGH SUNDAY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	57201-8134-44	5

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL NOTES