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T-M-4	03-15-07	STANDAND INTERSECTION PAVEMENT MARKINGS

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

HENDERSON COUNTY

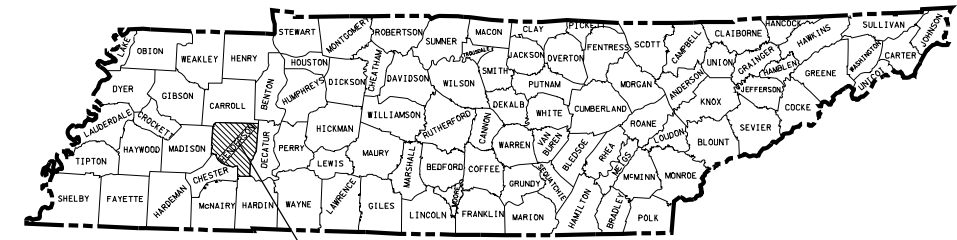
I - 40

FROM: L.M. 0.00 (MADISON COUNTY LINE)
TO: L.M. 6.08 (JOINT AT S.R. 104 ON RAMP)

RESURFACING

STATE HIGHWAY NO. I - 40 F.A.H.S. NO. N.A

TENN.	YEAR 2009	SHEET NO. 1
FED. AID PROJ. NO.	ARRA-IME-40-2(87)	
STATE PROJ. NO.	39001-8168-44	

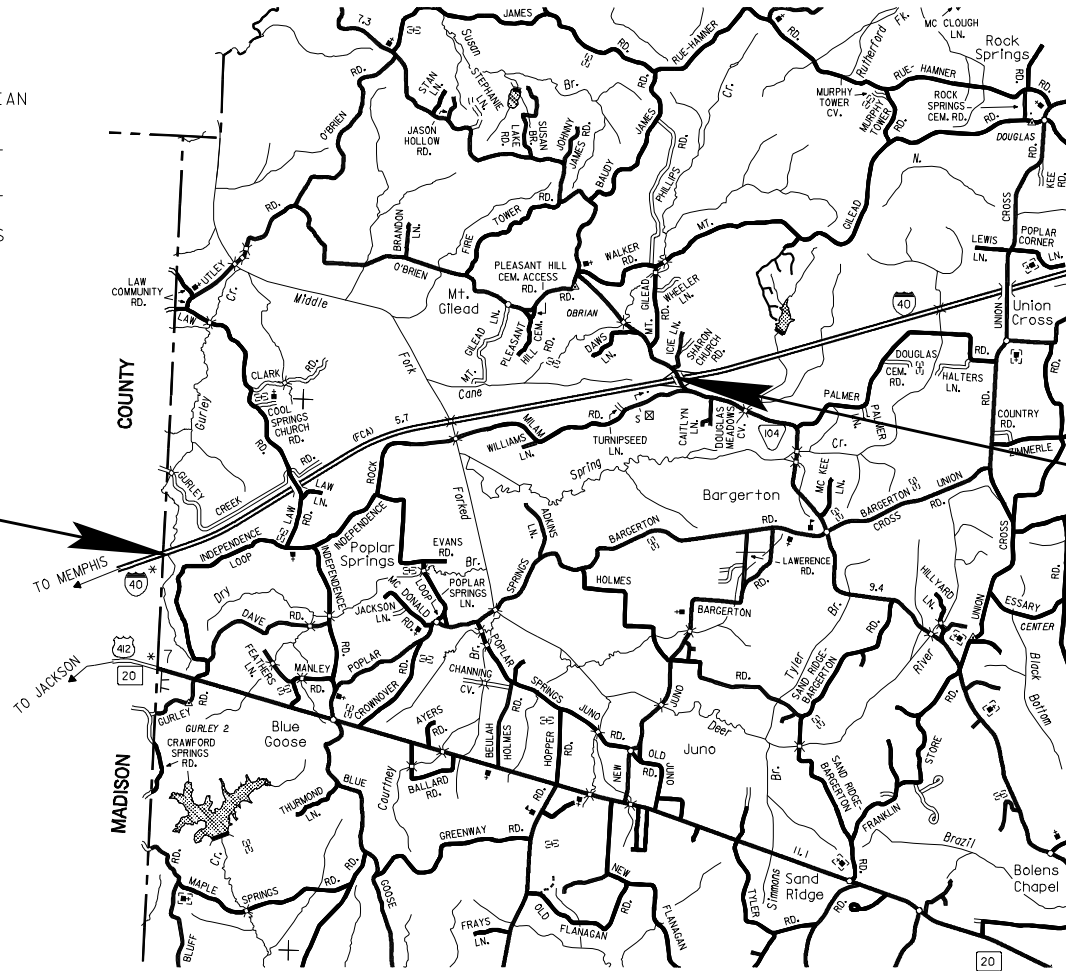


PROJECT SITE



BEGIN PROJECT L.M. 0.00
(MADISON COUNTY LINE)

END PROJECT L.M. 6.08
(JOINT AT S.R. 104 ON RAMP)



SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 2006 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

TDOT ROAD SP. SV. 2 MICHAEL I. RUSSELL, REG. 4

DESIGNER ROSE WILLIAMS/SCOTT PATE, REG. 4 CHECKED BY SCOTT PATE, REG. 4

P.E. NO. 98043-4175-04
PIN: 109911.01



PROJECT LENGTH 6.080 MILES

TRAFFIC DATA	
ADT (2009)	34860

APPROVED: *Paul D. Degees*
CHIEF ENGINEER

DATE:

APPROVED: *Scott F. Nield*
COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	ARRA-IME-40-2(87)	2

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
	202-03	REMOVAL OF RIGID PAVEMENT, SIDEWALK, ETC.	S.Y. 55
①	203-03	BORROW EXCAVATION (UNCLASSIFIED)	C.Y. 360
	203-06	WATER	M.G. 16
	209-08.03	TEMPORARY SILT FENCE (WITHOUT BACKING)	L.F. 4500
②	303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING "D"	TON 2049
③	307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON 26
	403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON 18
④	403-05.01	BITUMINOUS MATERIAL (FOG SEAL) SHOULDER	TON 65
⑤⑥	411-03.10	ACS MIX(PG76-22) GRADING D	TON 13374
⑦	415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y. 197721
⑧	705-04.07	TAN ENERGY ABSORBING TERM (NCHRP 350, TL3)	EACH 30
	712-01	TRAFFIC CONTROL	LS 1
⑨	712-06	SIGNS (CONSTRUCTION)	S.F. 1287
	712-08.03	ARROW BOARD (TYPE C)	EACH 4
	716-01.10	SNOWPLOWABLE REFLECTIVE MARKER	EACH 838
	716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH 838
	716-02.02	PLASTIC PAVEMENT MARKING (8" BARRIER LINE)	L.F. 1500
⑩	716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y. 30
	716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F. 100
⑪	716-02.10	PLASTIC PAVEMENT MARKING (6" LINE)	L.M. 30.76
⑫	716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M. 30.76
	717-01	MOBILIZATION	LS 1
	801-03	WATER (SEEDING & SODDING)	M.G. 9
	803-01	SODDING (NEW SOD)	S.Y. 6750

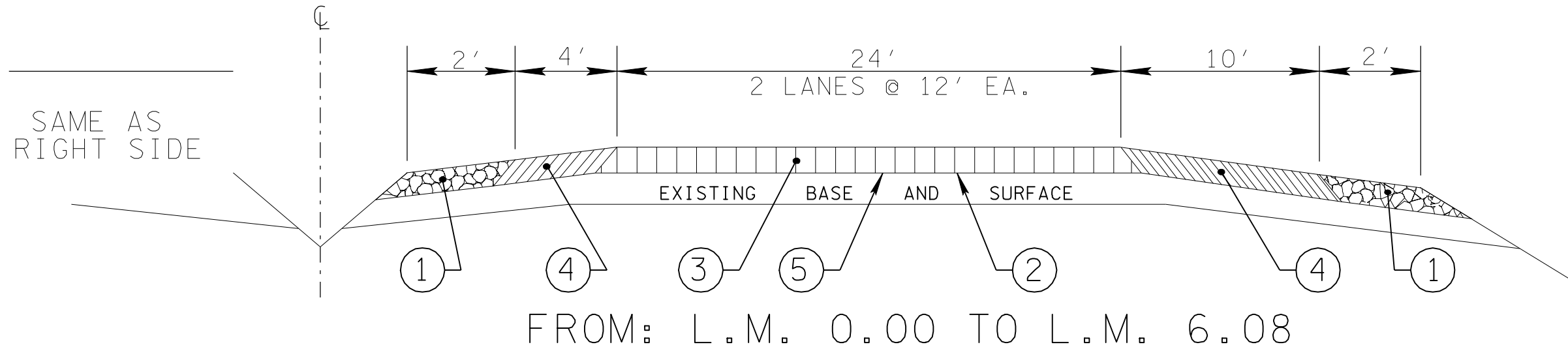
FOOTNOTES

- ① FOR GUARDRAIL TERMINAL EARTH BERM CONSTRUCTION
 - ② INCLUDES 38 TONS FOR BREAKOUT.
 - ③ FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03
 - ④ FOR USE ON INSIDE AND OUTSIDE SHOULDER ONLY
 - ⑤ INCLUDES 1832 TONS ADDITIONAL PAVEMENT ON RAMPS AND 200 TONS FOR SPOT LEVELING.
 - ⑥ SEE SPECIAL PROVISION NO. 411B FOR RIDEABILITY SPECIFICATIONS.
 - ⑦ INCLUDES 27,638 SQ. YD. OF COLD PLANING ON RAMPS
 - ⑧ FOR UPGRADE OF EXISTING ANCHORS AT ALL STRUCTURE LOCATIONS IN PROJECT LIMITS. THE COST OF REMOVAL OF EXISTING GUARDRAIL AND ANCHORS TO BE INCLUDED IN THIS ITEM.
 - ⑨ QUANTITY INCLUDES
 - 4 ROAD WORK NEXT 6 MI. (G20-1)
 - 4 END ROAD WORK (G20-2A)
 - 16 UNEVEN LANES (W8-11)
 - 4 ROAD WORK 1 MILE (W20-1)
 - 8 ROAD WORK AHEAD (W20-1)
 - 4 ROAD WORK 1500 FT. (W20-1)
 - 4 ROAD WORK 1000 FT. (W20-1)
 - 4 ROAD WORK 500 FT. (W20-1)
 - 4 ONE LANE ROAD AHEAD (W20-4)
 - 4 RIGHT LANE CLOSED 1,500 FT. (W20-5)
 - 4 RIGHT LANE CLOSED 1,000 FT. (W20-5)
 - 4 RIGHT LANE CLOSED 500 FT. (W20-5)
 - 4 LEFT LANE CLOSED 1,500 FT. (W20-5)
 - 4 LEFT LANE CLOSED 1,000 FT. (W20-5)
 - 4 LEFT LANE CLOSED 500 FT. (W20-5)
 - 4 FLAGGER AHEAD (W20-7a)
 - 4 FRESH OIL (W21-2)
 - 24 SHOULDER WORK (W21-5)
- THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.
- ⑩ FOR FINAL PAVEMENT MARKING ONLY.
 - ⑪ INCLUDES 3.40 L.M. FOR RAMPS.
 - ⑫ INCLUDES 3.40 L.M. FOR USE AS TEMPORARY LINE MARKINGS.

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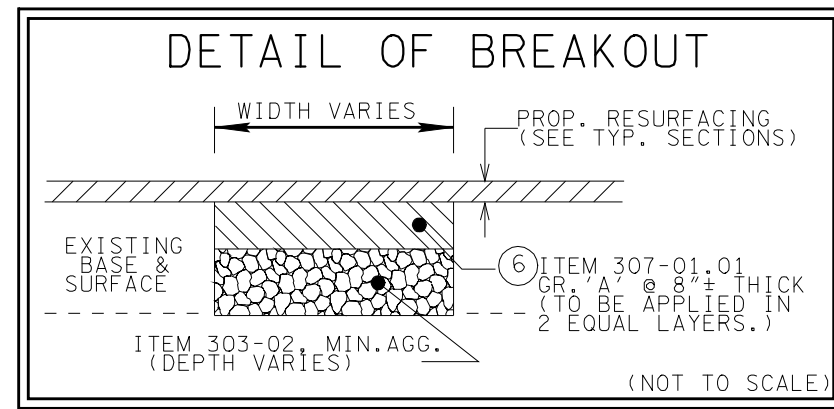
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	ARRA-IME-40-2(87)	3

TYPICAL SECTION



PROPOSED PAVEMENT SCHEDULE	
①	MINERAL AGGREGATE BASE @ 3"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D"
②	TACK COAT (TC) ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (AT 0.02 GAL./S.Y.)
③	ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 132.50 LBS./S.Y.) ITEM 411-03.10 ACS MIX (PG76-22) GRADING "D"
④	BITUMINOUS MATERIAL (FOG SEAL) SHOULDER ITEM 403-05.01 BITUMINOUS MATERIAL FOR SHOULDER FOG SEALING
⑤	COLD PLANING @ 1.25"± THICK (APPROX. 131.25 LBS./S.Y.) ITEM 415-01.02 COLD PLANING BITUMINOUS PAVEMENT
⑥	BITUMINOUS BASE COURSE (BLACK BASE) @ 8"± THICK (APPROX. 920.0 LBS./S.Y.) ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING "A" (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)

BRIDGE NOTE
THE CONTRACTOR SHALL:
"LEAVE AS IS"
BRIDGES AT L.M. 3.29 (EB & WB) AND
L.M. 7.33 (1130')



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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	ARRA-IME-40-2(87)	4

GENERAL NOTES

SPECIAL NOTES

GRADING

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

CERTIFICATION FOR ALL BORROW PITS MUST BE OBTAINED IN ACCORDANCE WITH SUBSECTION 107.06 OF THE STANDARD SPECIFICATIONS.

GUARDRAIL

THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPE UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE A LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL.

UTILITIES

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

MISCELLANEOUS

NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

IF THERMOPLASTIC IS USED ON THE FINAL SURFACE, THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKING AFTER THE PAVING OPERATION IS COMPLETED. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-02.10 PLASTIC PAVEMENT MARKING (6" LINE), LIN. MI.

PAVEMENT - RESURFACING

THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL NOTES

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY. WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC. UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS THIRTY (30) FEET SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL

DIFFERENCE IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC. CAUSED BY BASE, PAVING OR RESURFACING:

DIFFERENCE IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.

- A. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- B. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- C. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- D. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN 7 WORKDAYS AFTER THE CONDITION IS CREATED.

EROSION CONTROL

ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE,

THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFF-SITE MIGRATION OR DEPOSIT OF SEDIMENT ON ROADWAYS USED BY THE GENERAL PUBLIC.

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.

ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. COST TO BE INCLUDED IN ITEM NO. 712-06.

IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION. THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-02.10.

ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

MATERIAL TRANSFER DEVICE SHALL BE USED FOR "D" MIX.

ONLY 4 ARROW BOARDS WILL BE PAID FOR DURING THE LIFE OF THIS PROJECT. IF THE CONTRACTOR'S WORK PHASES REQUIRED ADDITIONAL ARROW BOARDS, THEIR COST WILL BE INCLUDED IN THE COST OF OTHER ITEMS.

TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITION ONLY:

- A: THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.
- B: THE SURFACE SHALL BE SWEEPED AND CLEANED ON ALL LOOSE MATERIALS.
- C: THE DIFFERENCE IN ELEVATION BETWEEN THE MILLED SURFACE AND THE ADJACENT LANE SHALL NOT EXCEED 1/8 INCHES.
- D: THE MILLED SURFACE SHALL BE PAVED WITHIN 48 HOURS.
- E: RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.
- F: ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGN (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS.
- G: IF RAVELING OR DETERIORATION OF THE MILLED SURFACE IS OCCURRING WHILE TRAFFIC IS DRIVING ON THE MILLED SURFACE, THEN THIS PRACTICE WILL NOT BE ALLOWED AND PAVING SHALL BE COMPLETED IMMEDIATELY AFTER MILLING.
- H: ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.

ALL TRAFFIC LANES SHALL BE OPEN TO TRAFFIC DURING, NON-WORK HOURS, NON-WORK DAYS, AND FROM 6:00 A.M. UNTIL 7:00 P.M. SUNDAY THRU FRIDAY, WITH NO WORK ON FRIDAY AND SATURDAY NIGHT.