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SBR-2-121	01-04-96	STRIP SEAL EXPANSION JOINTS - REPLACEMENT CONSTRUCTION DETAILS TYPE "J" - 1991

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

**BENTON**

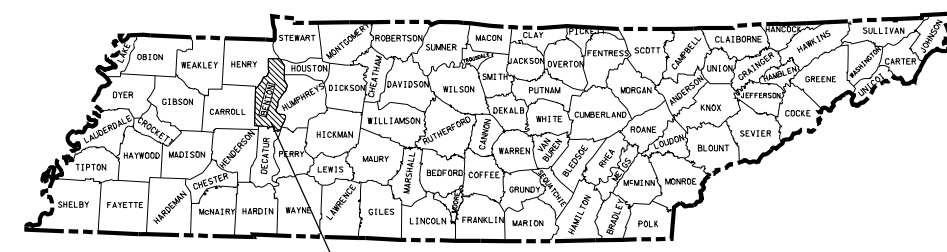
S.R. 147

FROM: L.M. 0.00 (JCT. S.R. 69A)  
TO: L.M. 5.36 (PINE GROVE RD.)

**RESURFACING**

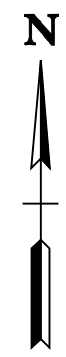
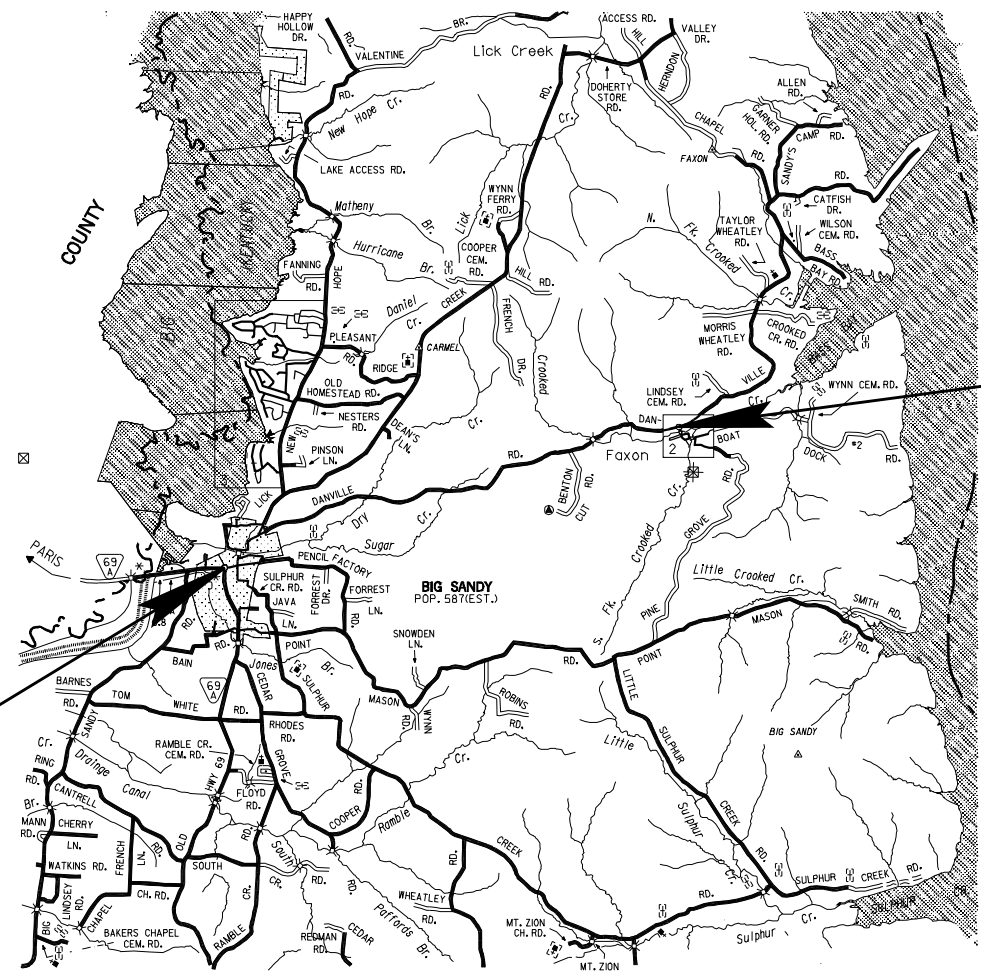
STATE HIGHWAY NO. F.A.H.S. NO. N.A

TENN.	YEAR 2009	SHEET NO. 1
FED. AID PROJ. NO.	ARRA-STP-147(2)	
STATE PROJ. NO.	03040-8215-14	



PROJECT SITE

UTILITY OWNERS	
UTILITY	UTILITY CONTACT
SEWER GAS	CITY OF BIG SANDY CHARLES BELYEW (901) 593-3213 P.O. BOX 176 BIG SANDY, TN. 38221



BEGIN PROJECT L.M. 00.00  
(JCT. S.R. 69A)

END PROJECT L.M. 5.36  
(PINE GROVE RD.)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 2006 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

TDOT ROAD SP. SV. 2 MICHAEL I. RUSSELL, REG. 4

DESIGNER DEREK LINK CHECKED BY SCOTT PATE

P.E. NO. 98043-4283-04  
PIN: 110830.01



PROJECT LENGTH 5.36 MILES

TRAFFIC DATA	
ADT (2009)	100

APPROVED: *Paul D. Degees*  
CHIEF ENGINEER

DATE:

APPROVED: *Scott F. Nield*  
COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION	
APPROVED:	
DIVISION ADMINISTRATOR	DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	03040-8215-14	2

### ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	340
203-06	WATER	M.G.	13
① 208-01.02	SHOULDER PREPARATION	L.M.	1
② 3 03-02	MINERAL AGGREGATE, TYPE B BASE, GRADING "C" OR "D"	TON	1678
3 07-01.01	ASPHALT CONCRETE MIX (PG 64-22) (B PMB-HM) GRADING A	TON	157
③ 3 07-01.08	ASPHALT CONCRETE MIX (PG 64-22) (B PMB-HM) GRADING B-M2	TON	167
④ ⑤ 3 07-01.10	ASPHALT CONCRETE MIX (PG 64-22) (B PMB-HM) GRADING C-W	TON	7140
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	8
415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	8510
⑥ 604-10.48	EXPANSION JOINT REPAIRS (TYPE J)	L.F.	64
604-10.50	BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB)	S.Y.	20
607-3 9.02	18" PIPE CULVERT (SIDE DRAIN)	L.F.	30
⑥ 617-01	BRIDGE DECK SEALANT	S.Y.	315
712-01	TRAFFIC CONTROL	LS	1
⑦ 712-06	SIGNS (CONSTRUCTION)	S.F.	1096
716-02.02	PLASTIC PAVEMENT MARKING (8" BARRIER LINE)	L.F.	600
716-02.04	PLASTIC PAVEMENT MARKING (CHANNELIZATION STRIPING)	S.Y.	25
716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	200
716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	1
716-04.01	PLASTIC PAVEMENT MARKING (STRAIGHT-TURN ARROW)	EACH	1
716-04.04	PLASTIC PAVEMENT MARKING (STRAIGHT ARROW)	EACH	3
716-04.10	PLASTIC PAVEMENT MARKING (HANDICAP SYMBOL)	EACH	1
⑧ 716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	30
⑨ 716-05.08	PAINTED PAVEMENT MARKING (PARKING LINE)	L.F.	1036
717-01	MOBILIZATION	LS	1

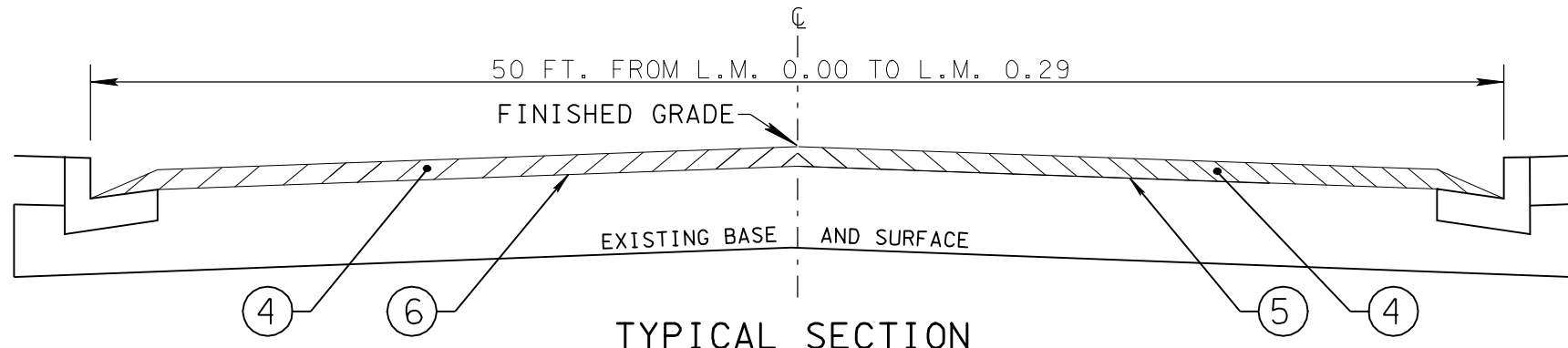
### FOOTNOTES

- ① ITEM WILL COVER THE COST OF NOTCHING FOR WIDENING OF ROADWAY FROM 18' TO 20' FROM L.M. 4.83 TO L.M. 5.36 ONLY.
- ② INCLUDES 231 TONS FOR BREAKOUT.
- ③ FOR WIDENING OF ROADWAY FROM 18' TO 20' FROM L.M. 4.83 TO L.M. 5.36 ONLY
- ④ INCLUDES 930 TONS FOR COUNTY ROADS, PRIVATE DRIVES, BUSINESS FRONTAGE, ADDITIONAL PAVEMENT AND 600 TONS FOR SPOT LEVELING.
- ⑤ WILL INCLUDE A MINIMUM OF 2 SEWER MANHOLES AND 2 GAS VALVES TO BE ADJUSTED.
- ⑥ REFER TO PAGES 5 AND 6 IN PLANS.
- ⑦ QUANTITY INCLUDES
  - 2 ROAD WORK NEXT 6 MI. (G20-1)
  - 2 END ROAD WORK (G20-2A)
  - 30 UNEVEN LANES (W8-11)
  - 17 ROAD WORK AHEAD (W20-1)
  - 2 ROAD WORK 1500 FT. (W20-1)
  - 2 ROAD WORK 1000 FT. (W20-1)
  - 2 ROAD WORK 500 FT. (W20-1)
  - 2 ONE LANE ROAD AHEAD (W20-4)
  - 2 FLAGGER AHEAD (W20-7a)
  - 2 FRESH OIL (W21-2)
  - 12 SHOULDER WORK (W21-5)
  - 30 SHOULDER DROP-OFF (W8-9a)

THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.
- ⑧ AN ADDITIONAL 7.8 MILES FOR TEMPORARY LINE MARKING WILL BE REQUIRED FOR COLD PLANING AND SPOT LEVELING.
- ⑨ THERE ARE 36 PARKING SPACES, 10 OF WHICH ARE PARALLEL PARKING SPACES.

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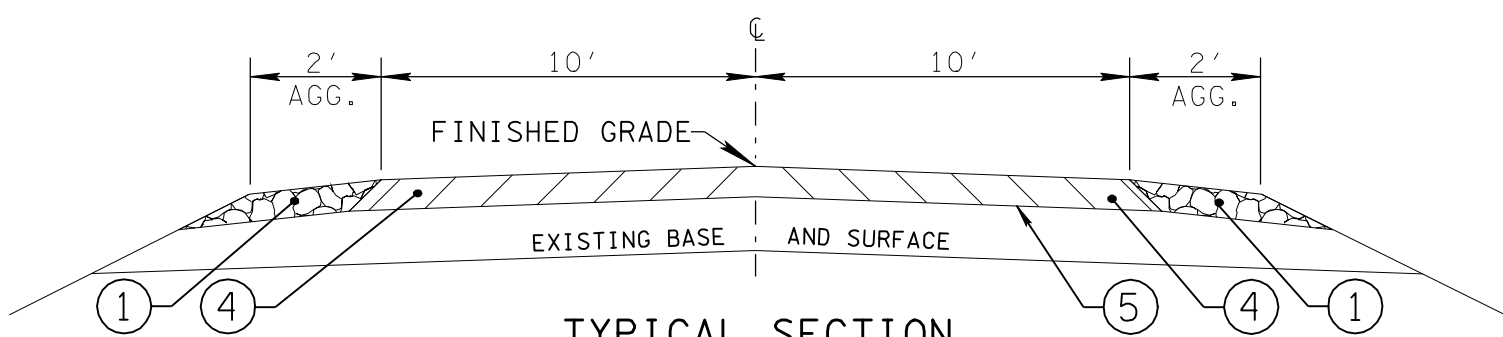
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**TYPICAL SECTION**  
FROM: L.M. 0.00 TO L.M. 0.29

**BRIDGE NOTE**

THE CONTRACTOR SHALL:  
REPAIR JOINT ONLY ON EXISTING STRUCTURE AT SUGAR CREEK. L.M. 0.27 BOTH ENDS  
"SEE SHEETS NO. 5 & 6 FOR DETAILS"

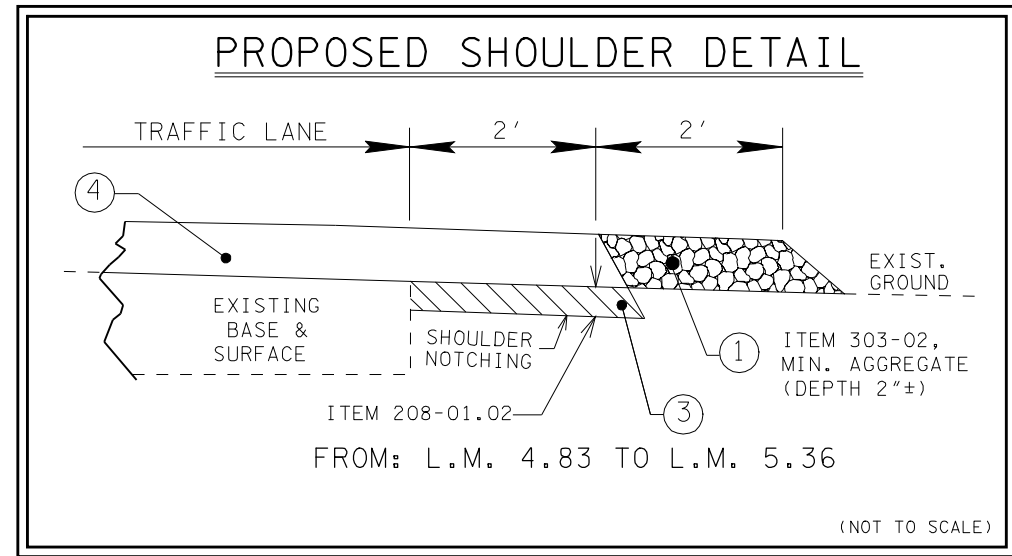
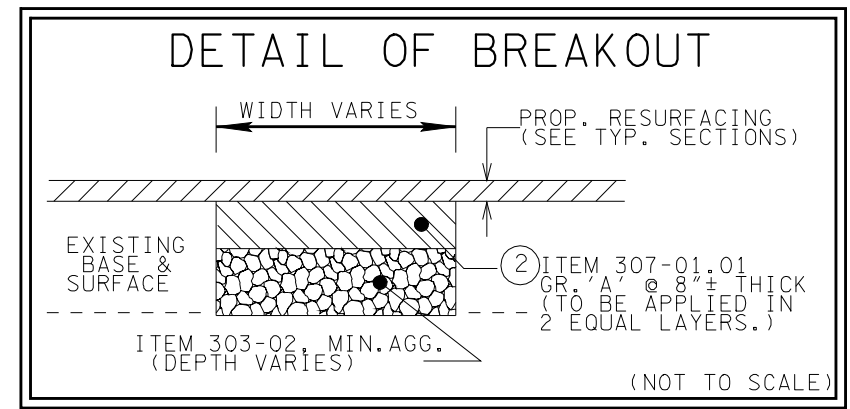


**TYPICAL SECTION**  
FROM: L.M. 0.29 TO L.M. 5.36

**BRIDGE NOTE**

THE CONTRACTOR SHALL:  
PAVE ACROSS EXISTING STRUCTURES WITH PLANS QUANTITIES AT L.M. 1.83 (30') & 4.40 (27')

PROPOSED PAVEMENT SCHEDULE	
①	MINERAL AGGREGATE BASE @ 2"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D"
②	BITUMINOUS BASE COURSE (BLACK BASE) @ 8"± THICK (APPROX. 920.0 LBS./S.Y.) ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "A" (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)
③	BITUMINOUS COURSE (BINDER) @ 2"± THICK (APPROX. 226.0 LBS./S.Y.) ITEM 307-01.08 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "B-M2"
④	BITUMINOUS COURSE (LEVELING/WEARING) @ 1.5"± THICK (APPROX. 165.00 LBS./S.Y.) ITEM 307-01.10 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "C-W"
⑤	TACK COAT (TC) ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (AT 0.02 GAL./S.Y.)
⑥	COLD PLANING @ 1.5"± THICK (APPROX. 158.00 LBS./S.Y.) ITEM 415-01.02 COLD PLANING BITUMINOUS PAVEMENT



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# GENERAL NOTES

# SPECIAL NOTES

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TENNESSEE D. O. T.
DESIGN DIVISION
FILE NO.

## GRADING

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE, OR LOCAL AGENCY.

## UTILITIES

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

## MISCELLANEOUS

THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.

NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

## PAVEMENT MARKINGS

TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS BEFORE DARK HOURS. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01 PAINTED PAVEMENT MARKINGS (4" LINE), LIN. MI.

WIDE (8 INCH) TEMPORARY PAVEMENT MARKING LINE WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-02.02 PAINTED PAVEMENT MARKING (8" BARRIER LINE), LIN. FT.

## PAVEMENT - RESURFACING

WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTIONS.

ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.

ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.

ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.

IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

PERMANENT PAVEMENT LINE MARKINGS SHALL BE REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01 PAINTED MARKING (4" LINE), LIN. MI.

## CONSTRUCTION WORK ZONE & TRAFFIC CONTROL NOTES

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY. WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC. UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS THIRTY (30) FEET SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

## PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL

DIFFERENCE IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC. CAUSED BY BASE, PAVING OR RESURFACING:

DIFFERENCE IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.

- A. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- B. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- C. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- D. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN 7 WORKDAYS AFTER THE CONDITION IS CREATED.

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.

ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. COST TO BE INCLUDED IN ITEM NO. 712-06.

IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION. THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.01.

THE CONTRACTOR WILL BE REQUIRED TO ESTABLISH AND LOCATE ALL NO-PASSING ZONES AS WELL AS PROVIDING THE LAYOUT OF ALL PAINTED PAVEMENT MARKINGS REQUIRED ON THE PROJECT. THE ESTABLISHMENT OF NO-PASSING ZONES IS TO BE IN ACCORDANCE WITH CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. ALL NO-PASSING ZONES WILL BE APPROVED BY THE ENGINEER WITH ADVICE FROM THE REGIONAL TRAFFIC ENGINEER PRIOR TO THE PAINT APPLICATION.

ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

THE CONTRACTOR WILL BE REQUIRED TO CLEAN AND BLADE GRASS FROM SHOULDERS BEFORE PLACING ITEM 303-02. MATERIAL BLADED FROM SHOULDERS TO BE PICKED UP AND DISPOSED OF AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN ITEM 303-02.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION, ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL PROPERLY ADJUST ALL MANHOLES, ALL UTILITY VALVE COVERS AND LIKE STRUCTURES TO THE FINISHED GRADE OF THE PAVEMENT. IF EXTENSION RINGS ARE USED, THEY WILL BE SPOT WELDED A MINIMUM OF FIVE TIMES. COST OF ADJUSTMENT IS TO BE INCLUDED IN ITEM 307-01.10, ADJUSTMENT IS TO BE MADE PRIOR TO THE LAYING OF ITEM 307-01.10.

BUSINESS ENTRANCES WILL BE PAVED THE SHOULDER WIDTH AND ONE PAVER WIDTH PLUS UP TO THREE FEET LIP OFF.

DRIVEWAYS AND FIELD ENTRANCES WILL BE PAVED THE SHOULDER WIDTH PLUS UP TO THREE FEET LIP OFF.

THIS POLICY DOES NOT APPLY TO BUSINESS ENTRANCES AND DRIVEWAYS BEHIND CURB AND GUTTER UNLESS OTHERWISE STATED IN THE PLANS OR DIRECTED BY THE ENGINEER.

THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 48 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED. EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.

THE TWO INCH NOTCH IS TO BE REPLACED WITH ITEM NO. 307-01.08, BM-2 MIX.


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TENNESSEE D.O.T.  
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FILE NO.

TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES							
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) SY	604-10.30 BRIDGE DECK REPAIRS (FULL DEPTH OF SLAB) SY	604-10.48 EXISTING EXPANSION JOINT REPAIR LF	617-01 BRIDGE DECK SEALANT SY	ITEM NUMBERS AND DETAIL FOR EXPANSION JOINT REPAIR
03-SR147-0.27 / SUGAR CREEK		BRIDGE DECK REPAIR BRIDGE DECK SEALANT EXPANSION JOINT REPAIR	20	0	64'-0"	315	ITEM NO. 604-10.48 DETAIL - TYPE "J" AT ABUT. 1 AND 2
TOTAL			20	0	64'-0"	315	

**E** FOR EXPANSION JOINT DETAILS SEE STD. DWG. NOS. SBR-2-115 THRU SBR-2-116 AND SBR-2-121. THE CONTRACTOR SHALL FIELD VERIFY ALL THE LENGTHS REQUIRED FOR INSTALLATION PRIOR TO JOINT FABRICATION. ALSO THE FOLLOWING SPECIAL PROVISIONS SHALL APPLY:  
105A - REGARDING APPROVAL OF SHOP DRAWINGS  
604S - REGARDING STRIP SEAL EXPANSION JOINTS

\* THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBARS AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), S.Y.

 WHEN BRIDGE DECK DRAINS ARE ENCOUNTERED, THE CONTRACTOR SHALL MAKE PROVISIONS FOR EXTENDING DRAINS TO TOP OF WEARING SURFACE. METHOD OF EXTENSION TO MEET WITH THE APPROVAL OF THE ENGINEER. COST TO BE INCLUDED IN BRIDGE DECK SEALANT ITEM NO. 617-01.

POWER DRIVEN HAND TOOLS :

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:  
(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 lb. CLASS SHALL NOT BE USED. ALSO TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC. (2) FULL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 lb. CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 lb. PNEUMATIC HAMMERS. (3) CHIPPING HAMMERS OF THE 15 lb. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

BRIDGE DECK REPAIR MATERIAL

IF OVERNIGHT LANE CLOSURES ARE PROVIDED FOR THIS PROJECT AND THE LANE CLOSURES ARE SUCH THAT TRAFFIC WILL BE KEPT OFF THE BRIDGE DECK WHERE DECK REPAIRS WILL BE MADE. THEN HIGH EARLY STRENGTH CONCRETE SHALL BE USED WITH A COMPRESSIVE STRENGTH OF 3,000 p.s.i. AT 18 HOURS. THE CONTRACTOR SHALL PROVIDE PROOF PRIOR TO BEGINNING WORK THAT PROPOSED CONCRETE MIX WILL OBTAIN REQUIRED PROPERTIES. PROOF SHALL BE PROVIDED BY AN INDEPENDENT TESTING COMPANY AND SUBMITTED TO THE MATERIAL AND TEST DIVISION OF TENNESSEE DEPARTMENT OF TRANSPORTATION FOR APPROVAL. THE MIX TO MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS CLASS "A". EXCEPT THE CEMENT SHALL BE A MINIMUM OF 714 lbs. A HIGH RANGE, WATER REDUCING ADMIXTURE (SUPERPLASTICIZER) SHALL BE USED SUBJECT TO THE APPROVAL OF AND THE CONCRETE SHALL ATTAIN A MINIMUM OF COMPRESSIVE STRENGTH OF 3,000 p.s.i. IN 18 HOURS. TRAFFIC SHALL NOT BE ALLOWED ON REPAIRED AREAS UNTIL THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF 18 HOURS

NOTE: PRIOR TO THE PLACEMENT OF THE BRIDGE DECK SEAL MEMBRANE, THE BRIDGE SHALL BE CLEANED OF ALL LOOSE DEBRIS AND EXCESS DIRT. THE BRIDGE DECK SHALL ALSO BE CLEANED OF ALL OIL, DIESEL OR ANY OTHER PETROLEUM PRODUCT SPILLS AND DRY ACCORDING TO THE MANUFACTURE'S SPECIFICATIONS.

THE WATERPROOFING MEMBRANE SHALL BE APPLIED ACCORDING TO THE MANUFACTURE'S SPECIFICATIONS. THE COST OF ALL THE MATERIALS NECESSARY FOR THE WATERPROOFING APPLICATION (WATERPROOFING MEMBRANE, PRIMER, MASTIC, ETC.) AND LABOR SHALL BE PAID FOR UNDER ITEM NO. 617-01 BRIDGE DECK SEALANT S.Y.

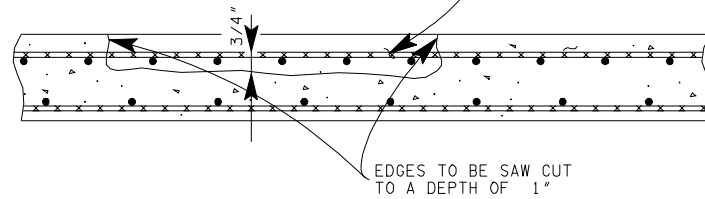
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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
**BRIDGE  
TABULATIONS  
&  
ESTIMATED  
QUANTITIES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	03040-8215-14	6

TENNESSEE D.O.T.  
DESIGN DIVISION  
FILE NO.

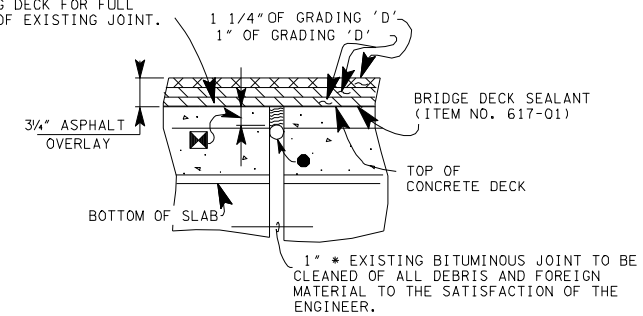
CONCRETE FOR DECK REPAIR SHALL BE HIGH EARLY CONCRETE WITH A COMPRESSIVE STRENGTH OF 3,000 p.s.i. AT 18 HOURS



**SKETCH SHOWING DECK REPAIR**

- NOTE: REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL. ALL REINFORCING STEEL IN AREAS OF DECK REPAIR SHALL BE COMPLETELY CLEANED. CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE OR INSTALLING PATCHING MATERIAL. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST THREE (3) DAYS IN ADVANCE.
- NOTE: ITEM NO. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), SHALL BE BID WITH THE CONTINGENCY THAT THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.
- NOTE: TRAFFIC CONTROL SHALL BE PROVIDED ON ROADWAY UNDERPASS DURING PARTIAL DEPTH DECK REPAIR OF OVERHEAD BRIDGE.

ONE LAYER OF DECK SEALANT (ITEM NO. 617-01) PLACED 2'-6" (\*) TRANSVERSE ACROSS EXISTING DECK FOR FULL LENGTH OF EXISTING JOINT.

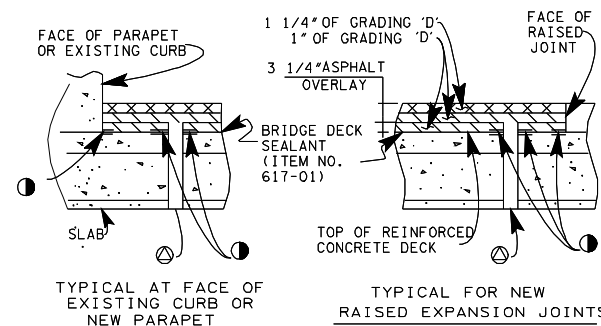


**SECTION THRU EXISTING BRIDGE SLAB JOINT**

(TYPICAL AT ALL EXISTING BITUMINOUS JOINTS WHEN NO JOINT REPAIR DETAIL IS SPECIFIED IN THE TABLE ABOVE)

- NOTE: THE EXISTING JOINT OPENING SHALL BE CAULKED WITH A BACKER ROD OF SUITABLE DIAMETER. THE ROD SHALL BE PLACED AT A DEPTH TO ENSURE THE CORRECT WIDTH/DEPTH RATIO OF THE NEW JOINT SEALANT MATERIAL. BACKER ROD SHALL BE AS PER JOINT MANUFACTURERS RECOMMENDATION.
- NOTE: THE EXISTING PREFORMED ELASTIC JOINT SEALER SHALL BE REMOVED AND THE CONCRETE SURFACES IN THE JOINT AREA SHALL BE BLAST CLEANED PRIOR TO THE INSTALLATION OF THE NEW JOINT SEALER. THE CONTRACTOR SHALL USE A COLD POUR SINGLE COMPONENT SILICONE JOINT SEALER AS APPROVED BY THE DIVISION OF MATERIALS AND TEST. COST OF JOINT CLEANING AND SEALING TO BE INCLUDED UNDER ITEM NO. 617-01 (BRIDGE DECK SEALANT). THE SEALER SHALL EXTEND UP AND ACROSS THE CURB OR SIDEWALK AREA TO THE OUTER EDGE OF THE BRIDGE SLAB.

- MASTIC AS RECOMMENDED BY MANUFACTURER OF MEMBRANE. SEE STD. SPEC. ART. 906.04.
- 1/2" Ø HOLE TO BE DRILLED BY THE CONTRACTOR NEAR FACE OF NEW PARAPETS OR EXISTING CURBS AT LOW POINTS.



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BRIDGE  
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