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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

BENTON

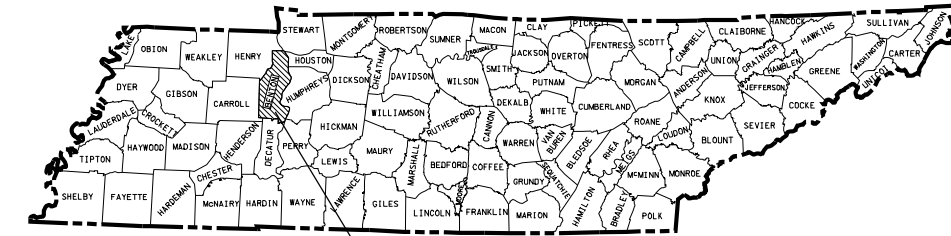
S.R. 391

FROM: L.M. 0.00 (JCT. S.R. 1)
TO: L.M. 5.31 (JCT. S.R. 1)

RESURFACING

STATE HIGHWAY NO. F.A.H.S. NO. N.A

TENN.	YEAR 2009	SHEET NO. 1
FED. AID PROJ. NO.	ARRA-STP-391(5)	
STATE PROJ. NO.	03002-8221-14	



PROJECT SITE



UTILITY OWNERS	
UTILITY	UTILITY CONTACT
WATER SEWER	CAMDEN WATERWORKS SUPT. JOHNNY TOWNSEND (731) 584-4656 110 HIGHWAY 641 S., P.O. BOX 779 CAMDEN, TN 38320
GAS	WEST TN. PUBLIC UTILITY DISTRICT GENE MASH (731) 986-8289 14055 PARIS ST. HUNTINGDON, TN. 38344

END PROJECT L.M. 5.31
(JCT. S.R. 1)

BEGIN PROJECT L.M. 0.00
(JCT. S.R. 1)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 2006 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

TDOT ROAD SP. SV. 2 MICHAEL I. RUSSELL, REG. 4

DESIGNER DEREK LINK CHECKED BY SCOTT PATE

P.E. NO. 98043-4283-04
PIN: 084920.01

SCALE: 0 1 2 3 MILES



PROJECT LENGTH 5.31 MILES

TRAFFIC DATA	
ADT (2009)	7833

APPROVED: *Paul D. Dejesus*
CHIEF ENGINEER

DATE:

APPROVED: *Scott F. Nief*
COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION	
APPROVED:	
DIVISION ADMINISTRATOR	DATE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	03002-8221-14	2

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	260
203-06	WATER	M.G.	9
① 303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING "C" OR "D"	TON	1110
307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	120
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	15
⑤ ④ ③ ② 411-02.10	ACS MIX (PG70-22) GRADING D	TON	113 87
⑥ 415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	16473 8
⑦ 604-10.44	EXPANSION JOINT REPAIRS	L.F.	122
⑧ 705-04.07	TAN ENERGY ABSORBING TERM (NCHRP 350, TL3)	EACH	14
712-01	TRAFFIC CONTROL	LS	1
⑨ 712-06	SIGNS (CONSTRUCTION)	S.F.	1452
716-02.01	PLASTIC PAVEMENT MARKING (4" LINE)	L.M.	33
716-02.02	PLASTIC PAVEMENT MARKING (8" BARRIER LINE)	L.F.	2105
716-02.04	PLASTIC PAVEMENT MARKING (CHANNELIZATION STRIPING)	S.Y.	350
716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	450
716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	49
⑩ 716-03.01	PLASTIC WORD PAVEMENT MARKING (ONLY)	EACH	4
716-03.03	PLASTIC WORD PAVEMENT MARKING (STOP AHEAD)	EACH	1
716-03.06	PLASTIC WORD PAVEMENT MARKING (SIGNAL AHEAD)	EACH	4
716-04.01	PLASTIC PAVEMENT MARKING (STRAIGHT-TURN ARROW)	EACH	2
716-04.05	PLASTIC PAVEMENT MARKING (STRAIGHT ARROW)	EACH	8
⑪ 716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	33
717-01	MOBILIZATION	LS	1
730-12.01	CONDUIT 1" DIAMETER (PVC)	L.F.	25
730-14.01	SHIELDED DETECTOR CABLE	L.F.	25
730-14.02	SAW SLOT	L.F.	710
730-14.03	LOOP WIRE	L.F.	1420

FOOTNOTES

- ① INCLUDES 176 TONS FOR BREAKOUT.
- ② INCLUDES 2918 TONS FOR COUNTY ROADS, PRIVATE DRIVES, BUSINESS FRONTAGE, ADDITIONAL PAVEMENT AND 100 TONS FOR SPOT LEVELING.
- ③ SHOULDERS TO BE PAVED WITH ROADWAY FROM L.M. 0.00 TO L.M. 1.46 AND FROM L.M. 3.31 TO L.M. 5.31.
- ④ WILL INCLUDE A MINIMUM OF 25 SEWER MANHOLES, 1 GAS VALVE AND 13 WATER VALVES TO BE ADJUSTED.
- ⑤ SEE SPECIAL PROVISION NO. 407G FOR FOR MATERIAL TRANSFER DEVICE.
- ⑥ INCLUDES 12667 S.Y. FOR BUSINESS ENTRANCES AND 25750 S.Y. FOR INTERSECTIONS AND RAMPS.
- ⑦ REFER TO PAGES 6 & 7 IN THE PLANS.
- ⑧ FOR UPGRADE OF EXISTING ANCHORS AT ALL STRUCTURE LOCATIONS IN PROJECT LIMITS. THE COST OF REMOVAL OF EXISTING GUARDRAIL AND ANCHORS TO BE INCLUDED IN THIS ITEM.
- ⑨ QUANTITY INCLUDES
 - 2 ROAD WORK NEXT 6 MI. (G20-1)
 - 2 END ROAD WORK (G20-2A)
 - 32 UNEVEN LANES (W8-11)
 - 37 ROAD WORK AHEAD (W20-1)
 - 2 ROAD WORK 1500 FT. (W20-1)
 - 2 ROAD WORK 1000 FT. (W20-1)
 - 2 ROAD WORK 500 FT. (W20-1)
 - 2 ONE LANE ROAD AHEAD (W20-4)
 - 2 FLAGGER AHEAD (W20-7a)
 - 2 FRESH OIL (W21-2)
 - 12 SHOULDER WORK (W21-5)
 - 32 SHOULDER DROP-OFF (W8-9a)
- THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.
- ⑩ FOR FINAL PAVEMENT MARKING ONLY. 6.2 MI. WILL BE REQUIRED FOR INTERSECTIONS
- ⑪ FOR USE AS TEMPORARY LINE MARKINGS OVER COLD PLANNED SURFACE. 6.2 MI. WILL BE REQUIRED FOR INTERSECTIONS.

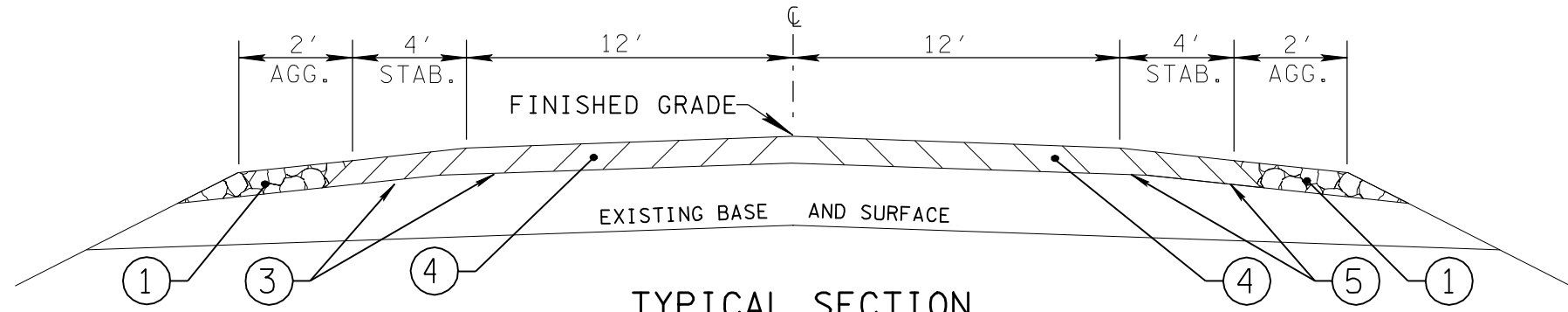
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SIGNALIZATION TABULATION

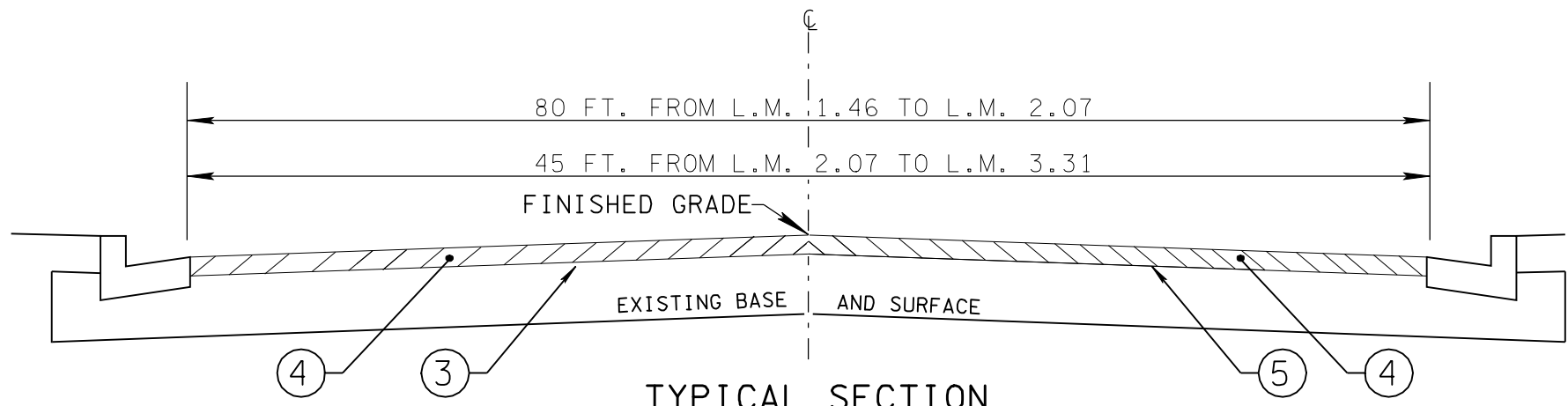
Intersection	Loop Wire	Saw Slot	Shielded	1" Conduit (PVC)
	(LF)	(LF)	Cable (LF)	(LF)
	730-14.03	730-14.02	730-14.01	730-12.01
SR 391 @ US 641	608	304	25	25
SR 391 @ Industrial	812	406	0	0
Totals	1420	710	25	25

Includes Side Street @ Industrial Dr
* Contractor needs to contact the TDOT Traffic Office for loop layout plans and special instructions.
731-935-0186

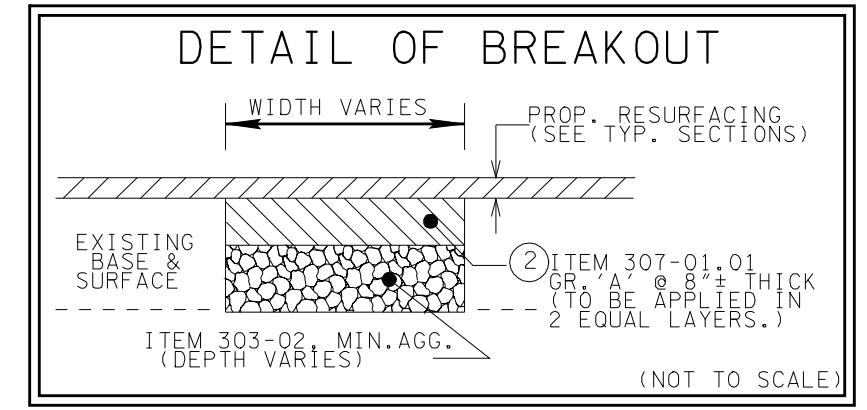
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	03002-8221-14	3



TYPICAL SECTION
 FROM: L.M. 0.00 TO L.M. 1.46
 FROM: L.M. 3.31 TO L.M. 5.31



TYPICAL SECTION
 FROM: L.M. 1.46 TO L.M. 2.07
 FROM: L.M. 2.07 TO L.M. 3.31



BRIDGE NOTE
 THE CONTRACTOR SHALL:
 PAVE ACROSS THE EXISTING STRUCTURES
 ON THIS PROJECT WITH PLANS QUANTITIES.

BRIDGE NOTE
 THE CONTRACTOR SHALL:
 REPLACE JOINT ON EXISTING STRUCTURES
 AT CANE CREEK. L.M. 3.26, CSX R.R.
 L.M. 3.45, AND CYPRESS CREEK L.M. 4.09
 BOTH ENDS.
 "SEE SHEET NO. 6 & 7 FOR DETAILS"

PROPOSED PAVEMENT SCHEDULE	
①	MINERAL AGGREGATE BASE @ 2"± THICK FOR SHOULDERS ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D"
②	BITUMINOUS BASE COURSE (BLACK BASE) @ 8"± THICK (APPROX. 920.0 LBS./S.Y.) ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "A" (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)
③	TACK COAT (TC) ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) (AT 0.02 GAL./S.Y.)
④	ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 132.50 LBS./S.Y.) ITEM 411-02.10 ACS MIX (PG70-22) GRADING "D"
⑤	COLD PLANING @ 1.25"± THICK (APPROX. 131.25 LBS./S.Y.) ITEM 415-01.02 COLD PLANING BITUMINOUS PAVEMENT

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 \\J04WF01\jack.tenn\04Shared\Design\RESURF REC4 PROJ\BENTON\SR391\LM0-5\SR391LM0-5 TYPICAL.dgn

GENERAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	03002-8221-14	4

TENNESSEE D. O. T.
DESIGN DIVISION
FILE NO.

GRADING

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY WITHOUT APPROVAL BY SAME. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE, OR LOCAL AGENCY.

GUARDRAIL

THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPE UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE A LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL.

UTILITIES

THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY CONTACTING THE UTILITY COMPANIES INVOLVED. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO CO-OPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED AROUND UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

MISCELLANEOUS

THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES WHERE AND AS DIRECTED BY THE ENGINEER.

NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS BEFORE DARK HOURS. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01 PAINTED PAVEMENT MARKINGS (4" LINE), LIN. MI.

IF THERMOPLASTIC IS USED ON THE FINAL SURFACE, THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKING AFTER THE PAVING OPERATION IS COMPLETED. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-02.01 PLASTIC PAVEMENT MARKING (4" LINE), LIN. MI.

PAVEMENT - RESURFACING

WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTIONS.

ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.

ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.

ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.

IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNALIZATION

ANY SIGNAL HEADS, WHEN VISIBLE TO DRIVERS BUT NOT OPERATIONAL, SHALL BE COMPLETELY COVERED.

THE PROJECT ENGINEER SHALL NOTIFY THE LOCAL GOVERNMENTAL AGENCY RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AT LEAST ONE DAY IN ADVANCE OF THE COLD PLANING ACTIVITY AT SIGNALIZED INTERSECTIONS WHERE DETECTOR LOOPS ARE ON THE PAVEMENT. THE MAINTAINING AGENCY WILL THEN BE RESPONSIBLE FOR DISCONNECTING THE LOOP DETECTORS AND MAKING ANY NECESSARY TIMING ADJUSTMENTS IN THE SIGNAL CONTROLLER PRIOR TO THE CONSTRUCTION.

THE PROJECT ENGINEER SHALL BE RESPONSIBLE FOR SUPPLYING THE CONTRACTOR WITH AS BUILT SIGNAL PLANS AT THE PRE-CONSTRUCTION CONFERENCE. THESE PLANS WILL PROVIDE THE CONTRACTOR WITH THE DESIRED LOCATION FOR DETECTOR LOOP REPLACEMENT.

LOOPS SHALL BE INSTALLED IN THE LEVELING COURSE IF LEVELING COURSE IS PROVIDED.

LOOPS REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL NOTES

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY. WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC. UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO BE PARKED WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS THIRTY (30) FEET SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL

DIFFERENCE IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC. CAUSED BY BASE, PAVING OR RESURFACING:

DIFFERENCE IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.

- A. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- B. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- C. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- D. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN 7 WORKDAYS AFTER THE CONDITION IS CREATED.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

SPECIAL NOTES

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.

ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. COST TO BE INCLUDED IN ITEM NO. 712-06.

IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.01.

THE CONTRACTOR WILL BE REQUIRED TO ESTABLISH AND LOCATE ALL NO-PASSING ZONES AS WELL AS PROVIDING THE LAYOUT OF ALL PAINTED PAVEMENT MARKINGS REQUIRED ON THE PROJECT. THE ESTABLISHMENT OF NO-PASSING ZONES IS TO BE IN ACCORDANCE WITH CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. ALL NO-PASSING ZONES WILL BE APPROVED BY THE ENGINEER WITH ADVICE FROM THE REGIONAL TRAFFIC ENGINEER PRIOR TO THE PAINT APPLICATION.

ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

THE CONTRACTOR WILL BE REQUIRED TO CLEAN AND BLADE GRASS FROM SHOULDERS BEFORE PLACING ITEM 303-02. MATERIAL BLADED FROM SHOULDERS TO BE PICKED UP AND DISPOSED OF AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN ITEM 303-02.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION, ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL PROPERLY ADJUST ALL MANHOLES, ALL UTILITY VALVE COVERS AND LIKE STRUCTURES TO THE FINISHED GRADE OF THE PAVEMENT. IF EXTENSION RINGS ARE USED, THEY WILL BE SPOT WELDED A MINIMUM OF FIVE TIMES. COST OF ADJUSTMENT IS TO BE INCLUDED IN ITEM 411-02.10, ADJUSTMENT IS TO BE MADE PRIOR TO THE LAYING OF ITEM 411-02.10.

MATERIAL TRANSFER DEVICE SHALL BE USED FOR ALL MIXES.

BUSINESS ENTRANCES AND DRIVEWAYS WILL BE PAVED THE SHOULDER WIDTH AND ONE PAVER WIDTH PLUS UP TO THREE FEET LIP OFF.

FIELD ENTRANCES WILL BE PAVED THE SHOULDER WIDTH PLUS UP TO THREE FEET LIP OFF.

THIS POLICY DOES NOT APPLY TO BUSINESS ENTRANCES AND DRIVEWAYS BEHIND CURB AND GUTTER UNLESS OTHERWISE STATED IN THE PLANS OR DIRECTED BY THE ENGINEER.

THE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE TRAFFIC LANE.

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-02.01.

ALL EXISTING SURPLUS MATERIAL AND JOINT MATERIAL ABOVE THE ADJACENT SURFACE ELEVATION WILL BE REMOVED TO THE NORMAL SURFACE LEVEL. COST TO BE INCLUDED IN OTHER ITEMS.

THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS TO ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

WHEN A PERFORMANCE GRADE (PG) ASPHALT MIX WITH PROPERTIES GREATER THAN THAT OF PG64-22 IS CALLED FOR ON RESURFACING PLANS AND IS THE ONLY ASPHALT GRADE ON THE PROJECT, THE CONTRACTOR HAS THE OPTION OF USING EITHER THE ASPHALT GRADE SHOWN ON THE PLANS OR AN ASPHALT GRADE EQUAL TO OR BETTER THAN PG64-22 FOR DRIVEWAYS AND BUSINESS ENTRANCES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE MATERIAL WILL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THE ASPHALT OR ASPHALT MIX. THE MATERIAL TICKETS SHALL BE MARKED "FOR DRIVEWAYS AND BUSINESS ENTRANCES ONLY" AT THE POINT OF MANUFACTURE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	03002-8221-14	5

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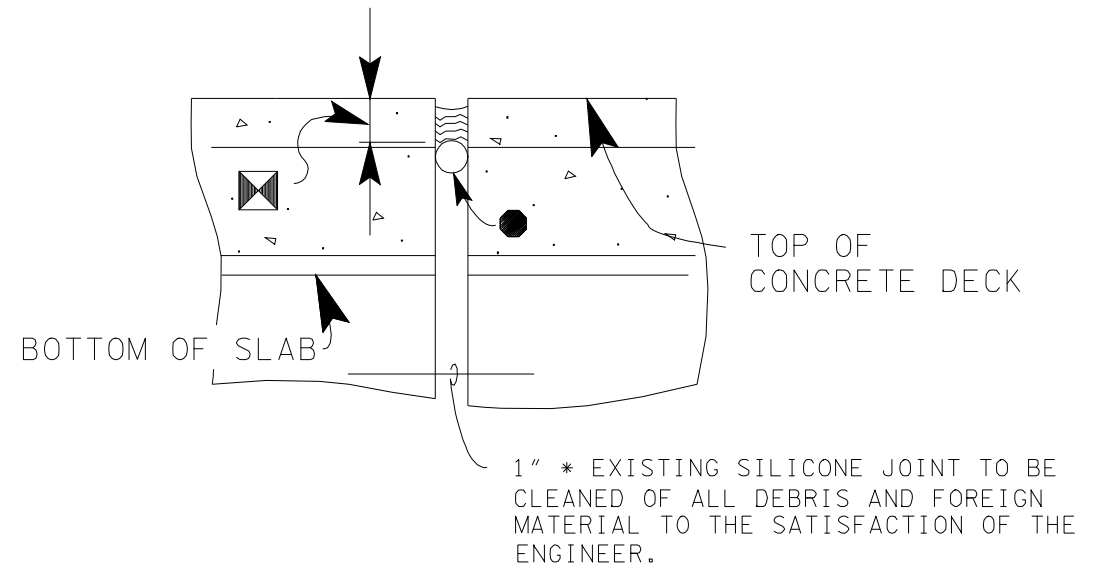
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	03002-8221-14	6

TABULATION OF BRIDGE RELATED WORK

LOCATION OF BRIDGE AND BRIDGE NUMBER	TYPE OF WORK	JOINT SEAL REPLACEMENT LF
03-SR391-3.26 / CANE CREEK	ASPHALT REMOVAL JOINT SEAL REPLACEMENT	38' - 4"
03-SR391-3.45 / CSX RAIL ROAD	JOINT SEAL REPLACEMENT	50' - 6"
03-SR391-4.09 / CYPRESS CREEK	JOINT SEAL REPLACEMENT	32' - 4"
TOTAL		121' - 2"



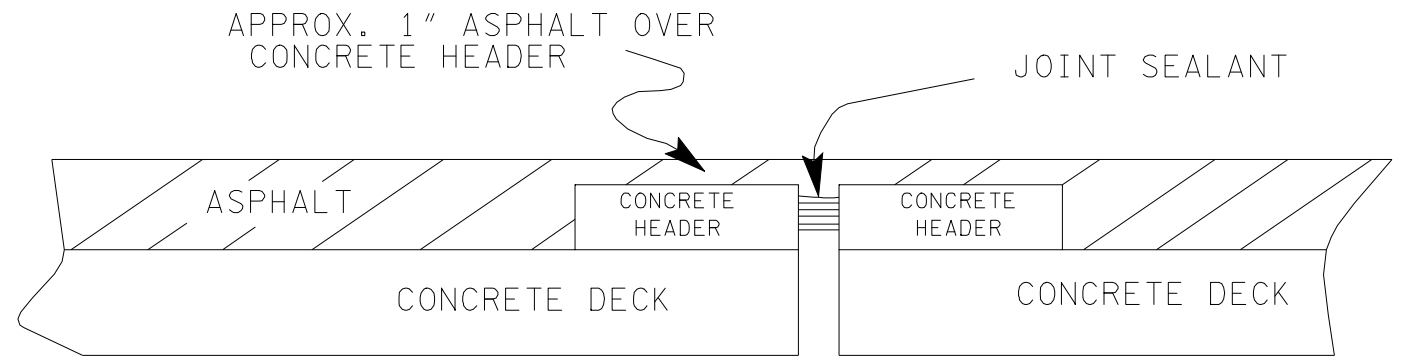
SECTION THRU EXISTING BRIDGE SLAB JOINT

- NOTE: THE EXISTING JOINT OPENING SHALL BE CAULKED WITH A BACKER ROD OF SUITABLE DIAMETER. THE ROD SHALL BE PLACED AT A DEPTH TO ENSURE THE CORRECT WIDTH/DEPTH RATIO OF THE NEW JOINT SEALANT MATERIAL. BACKER ROD SHALL BE AS PER JOINT MANUFACTURERS RECOMMENDATION.
- ⊠ NOTE: THE EXISTING PREFORMED ELASTIC JOINT SEALER SHALL BE REMOVED AND THE CONCRETE SURFACES IN THE JOINT AREA SHALL BE BLAST CLEANED PRIOR TO THE INSTALLATION OF THE NEW JOINT SEALER. THE CONTRACTOR SHALL USE A COLD POUR SINGLE COMPONENT SILICONE JOINT SEALER AS APPROVED BY THE DIVISION OF MATERIALS AND TEST. COST OF JOINT CLEANING AND SEALING TO BE INCLUDED UNDER ITEM NO. 604-10.44 (EXPANSION JOINT REPAIRS). THE SEALER SHALL EXTEND UP AND ACROSS THE CURB OR SIDEWALK AREA TO THE OUTER EDGE OF THE BRIDGE SLAB.

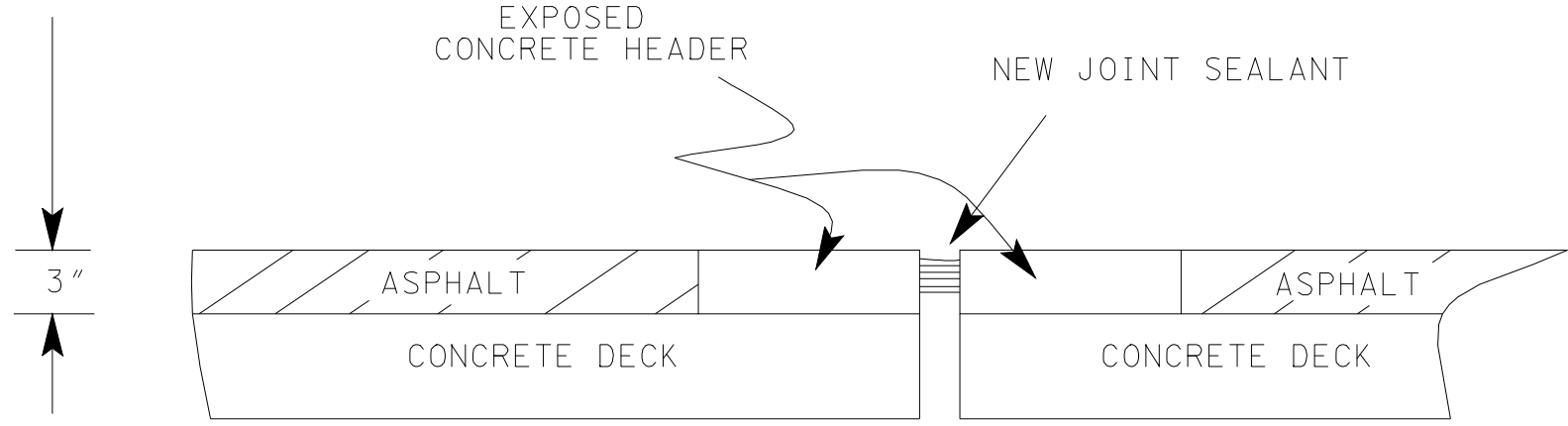
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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2009	03002-8221-14	7

BRIDGE No. 03-SR391-3.26



PRESENT JOINT @ BENT #2



PROPOSED JOINT @ BENT #2

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