

INDEX OF SHEETS  
 SEE SHEET NO. 1A FOR INDEX

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION  
 BUREAU OF ENGINEERING

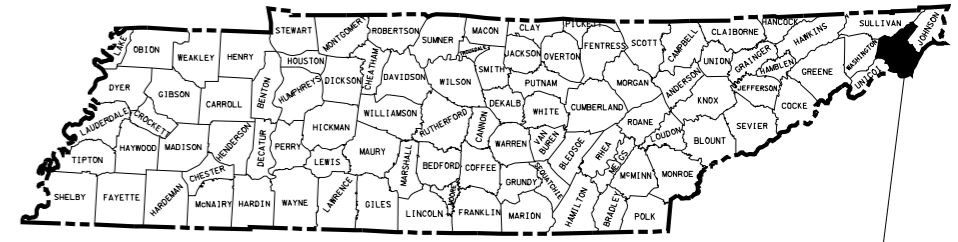
TENN.	YEAR	SHEET NO.
	2009	1
FED. AID PROJ. NO.	ARRA-STP-NHE-37(14)	
STATE PROJ. NO.	10003-8263-14	

**CARTER COUNTY**

S.R.37: FROM: APPROX. 210' N. OF CROOK ST. (L.M. 16.80)  
 TO: APPROX. BEGINNING OF BRIDGE OVER WATAUGA RIVER (L.M. 22.10)

**RESURFACING**

STATE HIGHWAY NO. 37 F.A.H.S. NO.

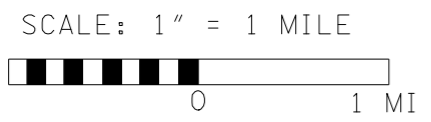


10003-8263-14  
 END ARRA-STP-NHE-37(14)  
 L.M. 22.10

CARTER CO.  
 S.R. 37

NO EQUATIONS  
 NO EXCLUSIONS

10003-8263-14  
 BEGIN ARRA-STP-NHE-37(14)  
 L.M. 16.80

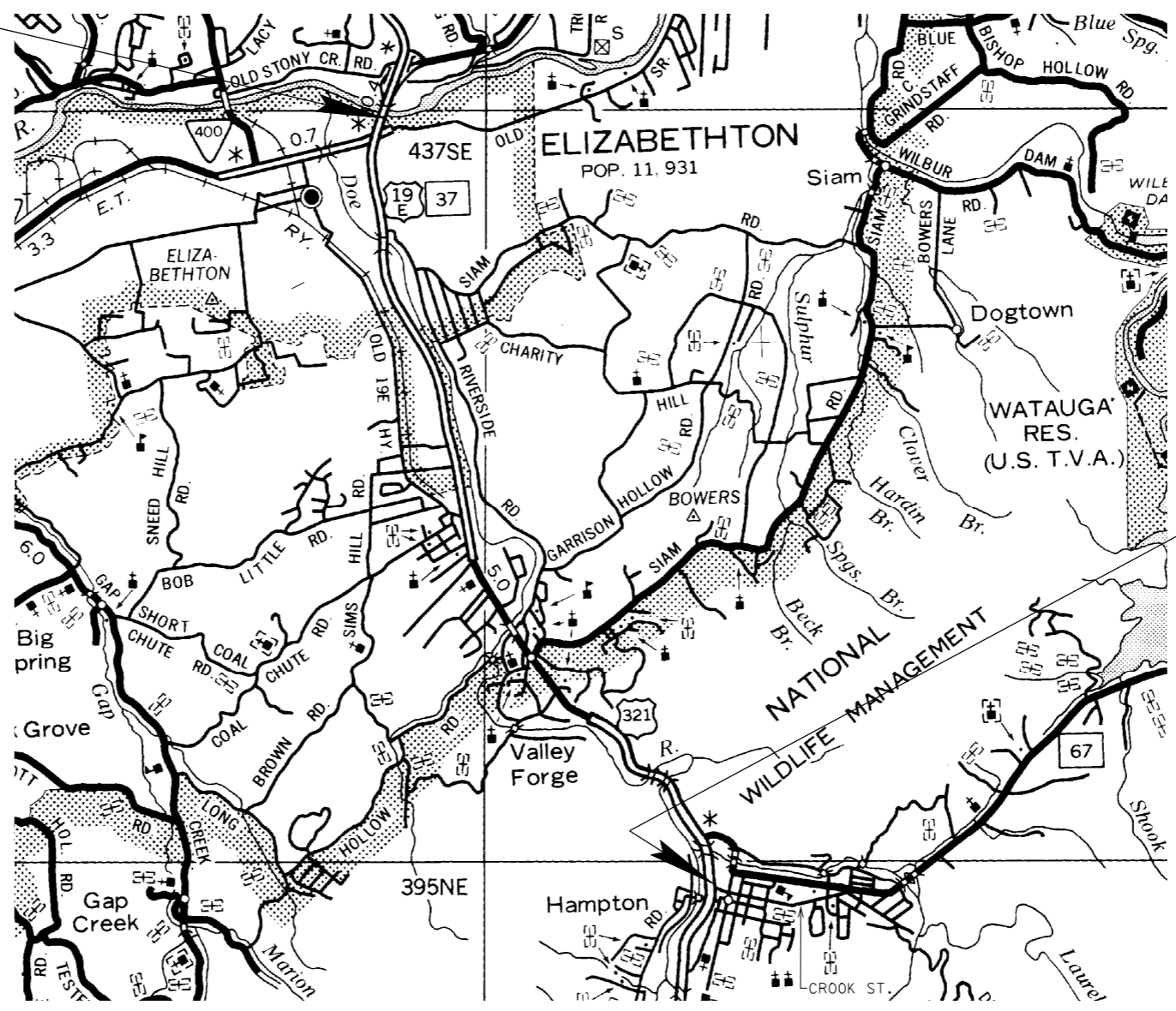


**SPECIAL NOTES**

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 2006 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

TDOT ROAD SP. SV. 1 ANDREA R. HALL  
 DESIGNER JOHN CULLOM, Jr.  
 P.E. NO. 98013-4203-04  
 PIN 085072.01



PROJECT LENGTH 5.30 MILES

TRAFFIC DATA	
ADT (2009)	21967
POSTED SPEED	45-50 MPH

APPROVED: Paul D. Duggan  
 CHIEF ENGINEER

DATE: \_\_\_\_\_  
 APPROVED: Sarah F. Nixey  
 COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION  
 FEDERAL HIGHWAY ADMINISTRATION

APPROVED: \_\_\_\_\_  
 DIVISION ADMINISTRATOR DATE

# INDEX OF SHEETS

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# STANDARD ROADWAY DRAWINGS

## TRAFFIC CONTROL APPURTENANCES

DWG.NO.	CURRENT REV. DATE	STANDARD TITLE
T-FAB-1	05-27-97	FLASHING YELLOW ARROW BOARD
T-M-1	04-15-04	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	09-05-04	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	09-19-91	MARKING STANDARDS FOR TRAFFIC ISLANDS, MEDIANS & PAVED SHOULDERS ON CONVENTIONAL ROADS
T-M-4	03-15-07	STANDARD INTERSECTION PAVEMENT MARKINGS
T-SG-2	07-29-04	LOOP LEAD-INS CONDUIT AND PULL BOXES
T-SG-3	11-11-04	NOTES AND DETAILS OF INDUCTIVE LOOPS

## EROSION PREVENTION AND SEDIMENT CONTROL

DWG.NO.	CURRENT REV. DATE	STANDARD TITLE
EC-STR-3B	04-01-08	SILT FENCE
EC-STR-3E	04-01-08	SILT FENCE FABRIC JOINING DETAILS
EC-STR-34	04-01-08	EROSION CONTROL BLANKET FOR SLOPE INSTALLATION

# STANDARD ROADWAY DRAWINGS

## ROADWAY DESIGN STANDARDS

DWG.NO.	CURRENT REV. DATE	STANDARD TITLE
RD-A-1	12-18-99	STANDARD ABBREVIATIONS
RD-L-1	10-26-94	STANDARD LEGEND
RD01-SE-2	10-15-02	URBAN SUPERELEVATION DETAILS
RD01-SE-3	10-15-02	RURAL SUPERELEVATION DETAILS

## SAFETY APPURTENANCES AND FENCE

DWG.NO.	CURRENT REV. DATE	STANDARD TITLE
S-GR-11	11-26-07	W-BEAM & THRIE BEAM BARRIER RAIL AND RUB RAIL ALTERNATES
S-GR-12	05-27-03	W-BEAM BARRIER POST DETAILS AND SPECIFICATIONS
S-GR-13	05-27-03	BARRIER RAIL MOUNTING, POST BLOCK-OUTS WITH VERTICAL ADJUSTMENT HOLES
S-GR-13A		BARRIER RAIL MOUNTING POST FOR PLASTIC BLOCK- OUTS WITH HORIZONTAL ADJUSTMENT HOLES
S-GR-14	09-05-98	W-BEAM BARRIER FASTENING HARDWARE AND BRIDGE APPROACH DELINEATORS
S-GR-15	06-30-05	W-BEAM BARRIER TERMINAL ELEMENT DETAILS
S-GR-38A	06-30-05	DETAILS FOR CONSTRUCTION OF ALTERNATE EARTH PAD FOR TYPE 38 GUARDRAIL END TERMINALS
S-GR-43		TANGENTIAL GUARDRAIL TERMINAL ANCHOR (TYPE 38) POST LAYOUT AND ERECTION DETAILS
S-GR-44		TANGENTIAL GUARDRAIL TERMINAL ANCHOR (TYPE 38) (2 TUBE) GUARDRAIL ELEMENT POST AND ASSEMBLY DETAILS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2009	ARRA-STP-NHE-37(14)	2

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FOOTNOTES

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
208-01	SHOULDERS AND DITCHES	L.M.	10.5
303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	3164
① 307-01.15	ACS MIX (PG64-22)(BPMLC-HM) GRADING CS	TON	3878
403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	37
② 411-01.07	ACS MIX (PG64-22) GRADING E (SHOULDER)	TON	7243
③ 411-02.10	ACS MIX (PG70-22) GRADING D	TON	13010
④ 411-10.01	SCORING PAVEMENT	L.M.	18
⑤ 415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	5227
⑥ 705-04.07	TAN ENERGY ABSORBING TERM (NCHRP 350, TL3)	EACH	9
⑦ 705-04.09	EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH	5
712-01	TRAFFIC CONTROL	L.S.	1
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	260
⑧ 712-06	SIGNS (CONSTRUCTION)	S.F.	1521
712-08.03	ARROW BOARD (TYPE C)	EACH	2
716-01.10	SNOWPLOWABLE REFLECTIVE MARKER	EACH	216
716-02.01	PLASTIC PAVEMENT MARKING (4" LINE)	L.M.	1
716-02.04	PLASTIC PAVEMENT MARKING (CHANNELIZATION STRIPING)	S.Y.	245
716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	350
716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	50
716-03.01	PLASTIC WORD PAVEMENT MARKING (ONLY)	EACH	2
⑨ 716-05.01	PAINTED PAVEMENT MARKING (4" LINE)	L.M.	37
⑩ 716-09.03	WET REFLECTIVE PAVEMENT MARKING (6" LINE)	L.M.	1
717-01	MOBILIZATION	L.S.	1
730-14.02	SAW SLOT	L.F.	1250
730-14.03	LOOP WIRE	L.F.	2500

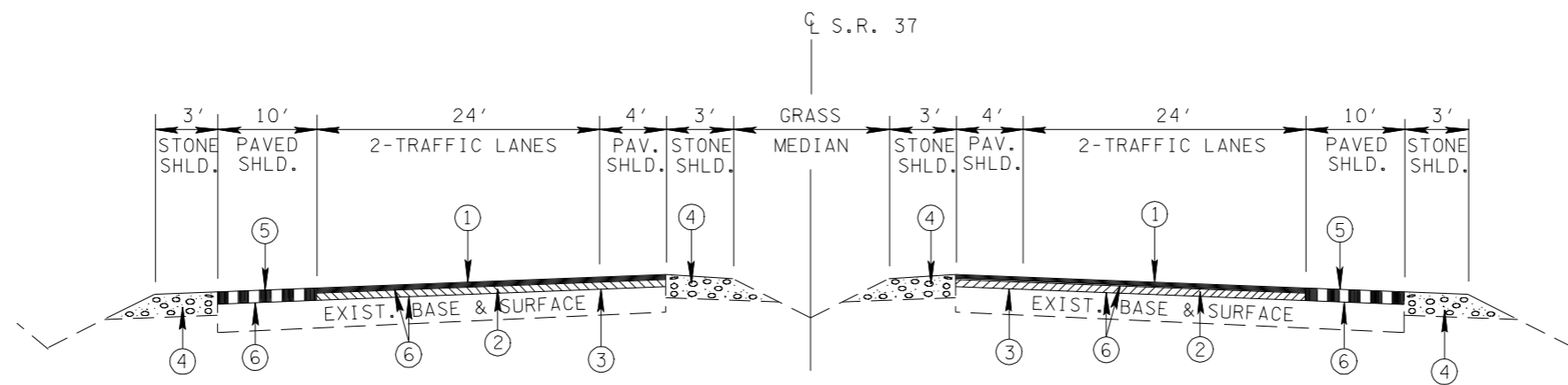
- ① INCLUDES 213 TONS FOR TURN LANES, 177 TONS FOR CROSSOVERS AND 135 TONS FOR SIDE STREETS.
- ② INCLUDES 150 TONS FOR BUSINESS ENTRANCES, 52 TONS FOR PRIVATE DRIVES AND 1060 TONS FOR SPOT LEVELING. (SPOT LEVELING TO BE USED ONLY TO CORRECT DEFICIENCIES ON EXISTING PAVEMENT AND IS NOT TO BE COMBINED WITH OTHER COURSES.)
- ③ INCLUDES 586 TONS FOR CROSSOVERS, 708 TONS FOR TURN LANES, 133 TONS FOR GORE AREA AND 450 TONS FOR SIDE STREETS. THE CONTRACTOR HAS THE OPTION OF USING EITHER THE PERFORMANCE GRADE ASPHALT OR REGULAR ASPHALT ON THE PAVING OF SIDE STREETS.
- ④ FOR EDGELINES INSIDE AND OUTSIDE OF DIVIDED SECTION (L.M. 17.01- L.M. 18.36 AND L.M. 19.41 - L.M. 21.73). FOR OUTSIDE EDGELINES OF UNDIVIDED SECTION (L.M. 18.36 - L.M. 19.41 AND L.M. 21.73 - L.M. 22.14). SCORING TO BE A RUMBLE STRIPE. SEE SPECIAL PROVISION 411RS, SPECIAL PROVISION 716ST AND DETAIL ON SHEET NO. 5. TO BE OMITTED AT DRIVES AND ENTRANCES OR AS DIRECTED BY THE T.D.O.T. SUPERVISOR.
- ⑤ INCLUDES 382 TONS FOR TURN LANES, 13 TONS FOR GORE AREA, 317 TONS FOR CROSSOVERS AND 246 TONS FOR SIDE STREETS.
- ⑥ TO BE USED IN THE FOLLOWING LOCATIONS OR AS DIRECTED BY THE T.D.O.T. SUPERVISOR.
 

L.M. 17.10 (LT. & RT.)	L.M. 18.36 (LT.)
L.M. 17.40 (RT.)	L.M. 18.68 (LT.)
L.M. 17.64 (LT.)	L.M. 18.77 (RT.)
L.M. 17.95 (LT.)	
L.M. 18.19 (LT.)	
- INCLUDES COST OF REMOVING EXISTING GUARDRAIL AND TERMINALS. REMOVED GUARDRAIL AND TERMINALS TO BECOME PROPERTY OF THE STATE MAINTENANCE FORCES UPON COMPLETION OF PROJECT.
- ⑦ INCLUDES ALL MATERIALS NECESSARY FOR CONSTRUCTION OF THE PAD AND ANY NECESSARY ESPC MEASURES.
- ⑧ QUANTITY INCLUDES THE FOLLOWING SIGNS:
 

<u>63</u> - ROAD WORK	(W20-1)
<u>14</u> - LANE CLOSED	(W20-5)
<u>8</u> - LANE ENDS	(W4-2)
<u>4</u> - ROAD WORK NEXT <u>5</u> MILES	(G20-1)
<u>4</u> - ADVISORY SPEED	(W13-1)
<u>4</u> - END ROAD WORK	(G20-2)
<u>27</u> - SHOULDER DROP-OFF	(W8-9A)
- ⑨ FOR TEMPORARY PAVEMENT MARKING ON INTERMEDIATE LAYERS.
- ⑩ TO BE USED ON CONCRETE BRIDGE DECKS. SHALL BE 3-M WET REFLECTIVE PAVEMENT MARKING LINE OR APPROVED EQUAL. THE PAVEMENT MARKING SHALL BE INSTALLED ACCORDING TO THE MANUFACTURE'S RECOMMENDATION AND SHALL INCLUDE SURFACE PREPARATION.

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RESURF	2009	ARRA-STP-NHE-37(14)	3

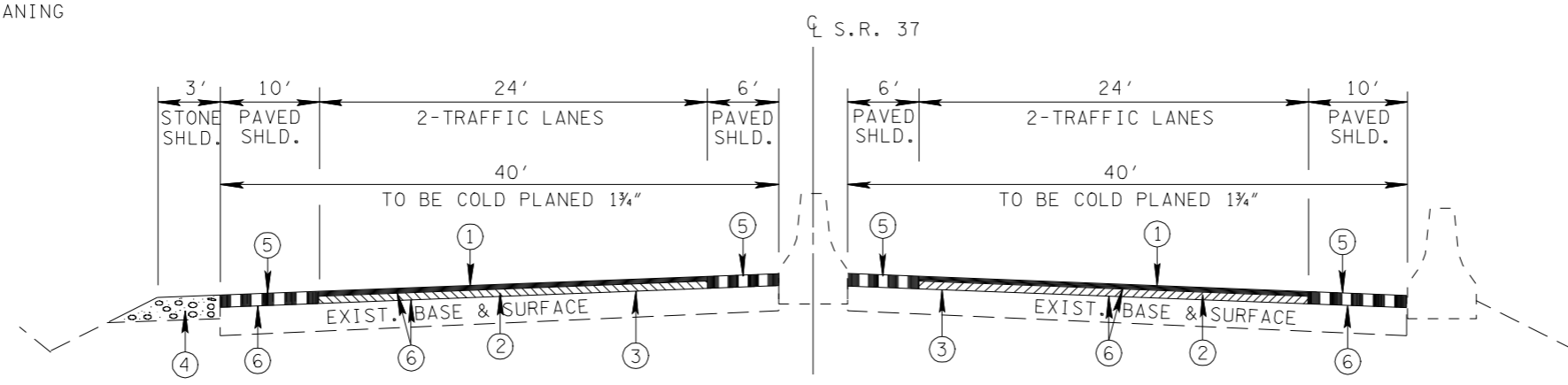
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**TYPICAL CROSS-SECTION OF IMPROVEMENT**

LOG MILE 16.80 TO LOG MILE 17.18  
LOG MILE 19.48 TO LOG MILE 21.93

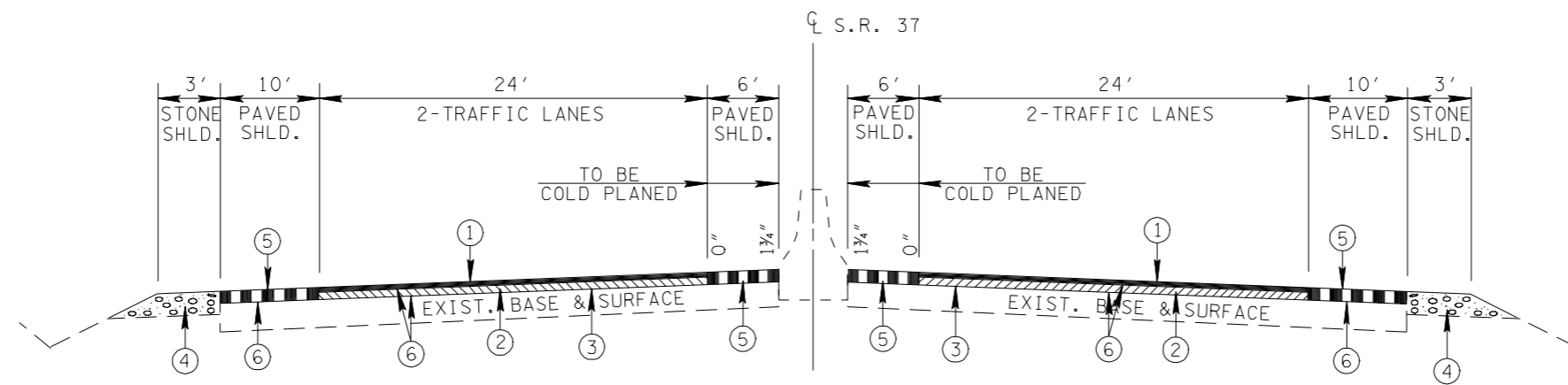
NOTE:  
ALL DEPTHS FOR COLD PLANING  
TO BE 1/2" TOLERANCE.



**TYPICAL CROSS-SECTION OF IMPROVEMENT**

LOG MILE 17.18 TO LOG MILE 17.52  
LOG MILE 17.52 TO LOG MILE 17.65 (EXIST. BARRIER WALL ON LEFT - NO STONE)  
LOG MILE 17.65 TO LOG MILE 17.80  
LOG MILE 17.97 TO LOG MILE 18.19

SEE SHEET 5 FOR  
PAVEMENT SCHEDULE



**TYPICAL CROSS-SECTION OF IMPROVEMENT**

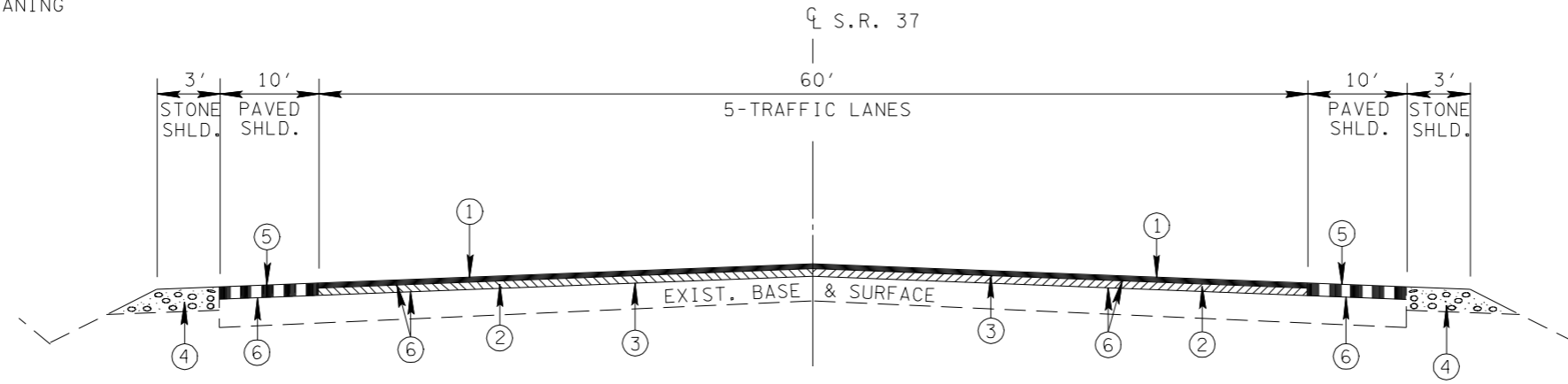
LOG MILE 17.80 TO LOG MILE 17.97

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NOTE:  
ALL DEPTHS FOR COLD PLANING  
TO BE 1/2" TOLERANCE.

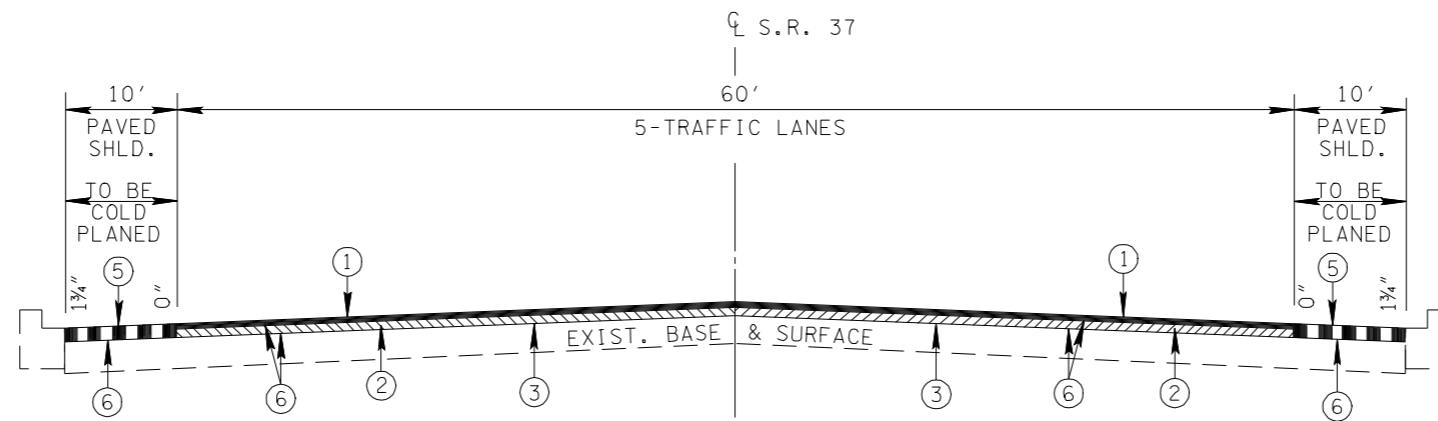
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RESURF	2009	ARRA-STP-NHE-37(14)	4

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**TYPICAL CROSS-SECTION OF IMPROVEMENT**

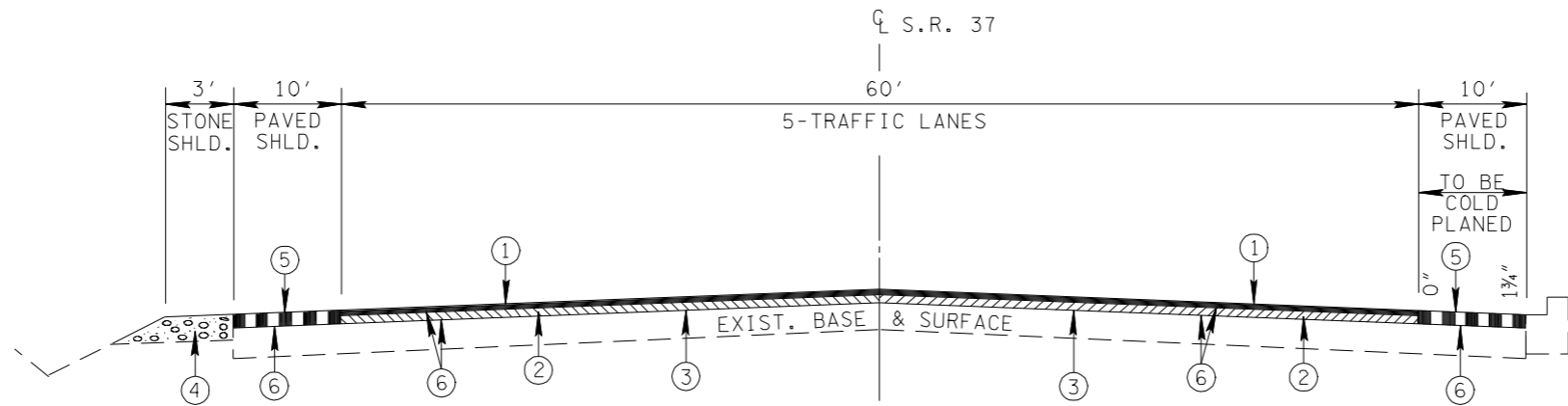
LOG MILE 18.19 TO LOG MILE 18.39



**TYPICAL CROSS-SECTION OF IMPROVEMENT**

LOG MILE 18.39 TO LOG MILE 18.54  
LOG MILE 18.75 TO LOG MILE 18.97 (NO COLD PLANING)  
LOG MILE 19.38 TO LOG MILE 19.48

SEE SHEET 5 FOR  
PAVEMENT SCHEDULE



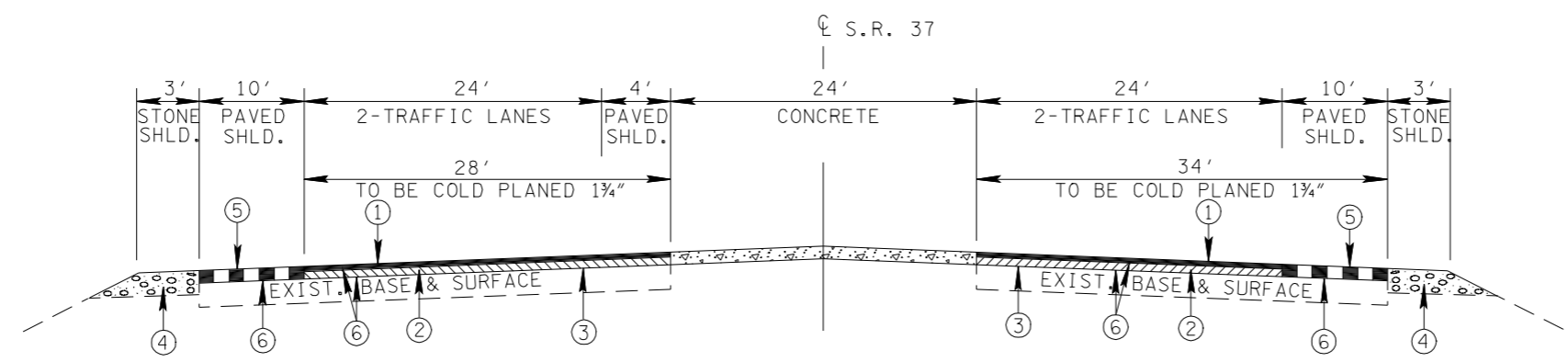
**TYPICAL CROSS-SECTION OF IMPROVEMENT**

LOG MILE 18.54 TO LOG MILE 18.75  
LOG MILE 18.97 TO LOG MILE 19.38 (EXIST. CURB & GUTTER ON LEFT - STONE ON RIGHT)

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2009	ARRA-STP-NHE-37(14)	5

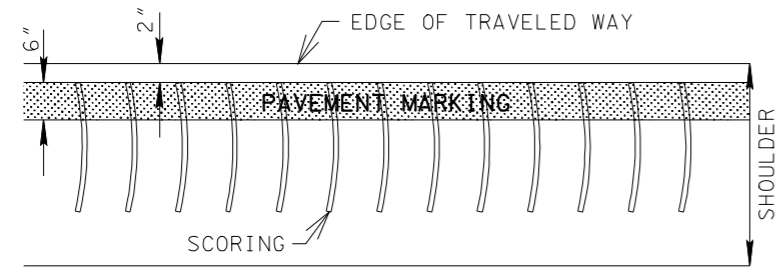
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NOTE:  
ALL DEPTHS FOR COLD PLANING  
TO BE 1/2" TOLERANCE.



**TYPICAL CROSS-SECTION OF IMPROVEMENT**

LOG MILE 21.93 TO LOG MILE 22.10



NOTE: SEE SPECIAL PROVISION 411RS AND SPECIAL PROVISION 716ST.

**DETAIL OF SCORING**

**PROPOSED PAVEMENT SCHEDULE**

①	ASPHALTIC CONCRETE SURFACE (HM) 1/4" TH. (APPROX. 132.5 LBS./SQ. YD.) 411-02.10 ACS MIX (PG70-22) GRADING D
②	BITUMINOUS PLANT MIX BASE (HM) APPROX. 40 LBS./SQ. YD.) 307-01.15 ACS MIX (PG64-22) (BPMLC-HM) GRADING CS
③	ASPHALTIC CONCRETE SURFACE (HM) (SPOT LEVELING) (APPROX. 200 TON/MILE) 411-01.07 ACS MIX (PG64-22) GRADING E (SHOULDER)
④	MINERAL AGGREGATE BASE - (SHOULDERS) 2" THICK 303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D
⑤	ASPHALTIC CONCRETE SURFACE (HM) 1 1/4" TH. (APPROX. 180 LBS./SQ. YD.) 411-01.07 ACS MIX (PG64-22) GRADING E (SHOULDER)
⑥	TACK COAT @ 0.02 GAL./SQ. YD. 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)

NOTE:  
NO PAVING REQUIRED ON BRIDGES OVER DOE RIVER (L.M. 17.43 LT.), (L.M. 17.56 RT.)  
AND (L.M. 18.82 LT. & RT.).

ITEM NO. 307-01.15 TO BE OMITTED ON BRIDGES OVER DOE RIVER  
(L.M. 21.29 RT.) AND (L.M. 21.29 LT.).

ITEM NO. 411-02.10 (D-MIX) IS USED THE FULL WIDTH OF BRIDGES OVER DOE RIVER  
(L.M. 21.29 RT.) AND (L.M. 21.29 LT.).

GENERAL NOTES

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UTILITIES

- (1) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (2) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106.

- (4) ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.
- (5) ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.
- (6) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

MISCELLANEOUS

- (1) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

SIGNALIZATION

- (1) EQUIPMENT AND INSTALLATION OF TRAFFIC SIGNALS SHALL COMPLY WITH TDOT STANDARD SPECIFICATIONS, SECTION 730.
- (2) THE PROJECT ENGINEER SHALL NOTIFY THE LOCAL GOVERNMENTAL AGENCY RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AT LEAST ONE DAY IN ADVANCE OF THE COLD PLANING ACTIVITY AT SIGNALIZED INTERSECTIONS WHERE DETECTOR LOOPS ARE IN THE PAVEMENT. THE MAINTAINING AGENCY WILL THEN BE RESPONSIBLE FOR DISCONNECTING THE LOOP DETECTORS AND MAKING ANY NECESSARY TIMING ADJUSTMENTS IN THE SIGNAL CONTROLLER PRIOR TO THE CONSTRUCTION.
- (3) THE PROJECT ENGINEER SHALL BE RESPONSIBLE FOR SUPPLYING THE CONTRACTOR WITH AS-BUILT SIGNAL PLANS AT THE PRE-CONSTRUCTION CONFERENCE. THESE PLANS WILL PROVIDE THE CONTRACTOR WITH THE DESIRED LOCATION FOR DETECTOR LOOP REPLACEMENT.
- (4) LOOPS SHALL BE INSTALLED IN THE LEVELING COURSE IF A LEVELING COURSE IS PROVIDED.
- (5) LOOP REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.

PAVEMENT MARKINGS

- (1) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS BEFORE DARK HOURS. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.01 PAINTED PAVEMENT MARKING (4" LINE), LIN. MI.
- (2) IF THERMOPLASTIC IS USED ON THE FINAL SURFACE, THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKING AFTER THE PAVING OPERATION IS COMPLETED. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-02.01 PLASTIC PAVEMENT MARKING (4" LINE), LIN. MI.

CONSTRUCTION WORK ZONE AND TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

PAVEMENT RESURFACING

- (1) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (2) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (3) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.

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RESURF	2009	ARRA-STP-NHE-37(14)	7

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(5) USE OF BARRICADES, PORTABLE BARRIER RAILS, VERTICAL PANELS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

(6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

(7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

EROSION PREVENTION AND SEDIMENT CONTROL  
LITTER, DEBRIS, WASTE, PETROLEUM

(1) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER, CONSTRUCTION DEBRIS, AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S.

(2) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION (NFPA). APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

(1) DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

(2) 1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES:

a. WARNING SIGNS, UNEVEN PAVEMENT (W8-11) AND/OR SHOULDER DROP-OFF (W8-9A), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

(3) 2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES. TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN PAVEMENT AND/OR LOW SHOULDER) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THRU TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3.5 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN PAVEMENT AND/OR LOW SHOULDER) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING TWO MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2009	ARRA-STP-NHE-37(14)	9

S.R. 37 CARTER CO.  
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(8) SPECIAL PROVISION NO. 411C APPLIES TO THIS PROJECT. AVERAGE HC-IRI NUMBER PER MILE IS 63.1. (EXCLUSIONS: L.M. 17.40 - L.M. 17.60), (L.M. 18.78 - L.M. 18.87), (L.M. 21.91 - L.M. 22.14). SEE TABLE BELOW.

NORTHBOUND S.R. 37		
FROM	TO	HC-IRI LANE 1
0.00	1.00	66
1.00	2.00	73
2.00	3.00	61
3.00	4.00	52
4.00	4.95	71

NORTHBOUND S.R. 37		
FROM	TO	HC-IRI LANE 2
0.00	1.00	70
1.00	2.00	68
2.00	3.00	59
3.00	4.00	47
4.00	4.95	71

SOUTHBOUND S.R. 37		
FROM	TO	HC-IRI LANE 1
0.00	1.00	68
1.00	2.00	49
2.00	3.00	62
3.00	4.00	63
4.00	4.95	65

SOUTHBOUND S.R. 37		
FROM	TO	HC-IRI LANE 2
0.00	1.00	77
1.00	2.00	53
2.00	3.00	59
3.00	4.00	66
4.00	4.95	62

(9) EXTREME CARE IS TO BE TAKEN WHEN COLD PLANING ADJACENT TO THE "WHITE TOPPED" LANES. THE "WHITE TOPPED" LANES ARE TO BE LEFT AS IS (NO COLD PLANING AND NO ASPHALT ADDED). ANY DAMAGE TO THESE AREAS IS TO BE REPAIRED AT THE CONTRACTORS EXPENSE.

CONSTRUCTION WORK ZONE AND TRAFFIC CONTROL

- (1) DURING ALL PERIODS OF CONSTRUCTION INACTIVITY, THE CONTRACTOR SHALL BE REQUIRED TO KEEP ALL LANES OPEN IN ACCORDANCE WITH THE TDOT PAVEMENT DROP-OFF POLICY. THIS WILL REQUIRE THE REMOVAL OF ALL TRAFFIC CONTROL DEVICES ASSOCIATED WITH ANY LANE CLOSURES AT THE END OF THE WORK DAY AND THE REINSTALLATION OF THE DEVICES AT THE BEGINNING OF THE NEXT WORK DAY.
- (2) THE CONTRACTOR SHALL KEEP ALL LANES OPEN IN BOTH DIRECTIONS ON ALL WEEKENDS (FRIDAYS, SATURDAY AND SUNDAY) THAT THERE IS A SPRINT CUP RACE IN BRISTOL.
- (3) ALL RESURFACING AND COLD PLANING OPERATIONS SHALL BE SUSPENDED BETWEEN THE HOURS OF 6:00 A.M. AND 9:00 A.M. AND BETWEEN THE HOURS OF 3:00 P.M. AND 6:00 P.M. DUE TO "PEAK HOUR TRAFFIC VOLUMES".
- (4) THE CONTRACTOR SHALL CLOSE NO MORE THAN ONE LANE IN EACH DIRECTION DURING HIS WORKING TIME AND ALL LANES ARE TO REMAIN OPEN DURING "PEAK HOURS".

EROSION PREVENTION & SEDIMENT CONTROL  
DISTURBED AREA

- (1) ALL DISTURBED AREAS SHALL BE PROPERLY STABILIZED AS SOON AS PRACTICABLE.

SEDIMENT CONTROL

- (1) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFF-SITE MIGRATION OR DEPOSIT OF SEDIMENT ON ROADWAYS USED BY THE GENERAL PUBLIC.

STREAM/WETLAND

- (1) WETLANDS AREAS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS.